
4.1 Aesthetics

4.1.1 Introduction

This section of the EIR evaluates the potential impacts of the proposed Project on visual character, views, light and glare, and shade and shadow. The evaluation of visual character impacts considers the existing visual character of the Project site and surrounding area and how implementation of the proposed LAX Northside Design Guidelines and Standards would affect this visual character. The evaluation of view impacts considers the existing viewsheds and known visual resources (including scenic highways and landmarks) which may be affected by the development of the Project site.

The analysis of light and glare assesses the effects of new sources of nighttime lighting and glare from the reflection of sunlight or artificial light from any reflective surface that would be established on the Project site. This section also evaluates patterns of shading that would be created by the maximum development of the proposed Project and the potential effect on any surrounding sensitive uses.

This analysis considers relevant sections of the LAX Master Plan and the City of Los Angeles Zoning and Municipal Code sections that are applicable to visual resources, while comparing them to the following relevant proposed LAX Northside Design Guidelines and Standards: land use, height, bulk, mass, setbacks, stepbacks, lighting, signage, and façade treatments.

4.1.2 Environmental Setting

4.1.2.1 Regulatory Framework

4.1.2.1.1 State

State Scenic Highways Program

The intent of the State of California Scenic Highway Program is to preserve and protect scenic highways. The nearest officially designated state scenic highway to the Project site is, State Route 2, approximately 14 miles northeast of the Project site.

4.1.2.1.2 Local

City of Los Angeles General Plan Transportation Element

The City of Los Angeles Transportation Element identifies Scenic Highways recognized by the City of Los Angeles. Vista del Mar, between Culver Boulevard in the Playa del Rey Community and south of West Grand Avenue near the boundary of the City of Los Angeles with the City of Manhattan Beach, is identified as a scenic highway.

City of Los Angeles Zoning and Municipal Code

The City of Los Angeles Zoning Code, Section 12.50, Airport Approach Zoning Regulations, establishes special airport zoning regulations for land uses within the approach zones of LAX (specifically within the areas mapped in the Airport Hazards Area Maps referenced in the Code)

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in order to prevent the creation or establishment of airport hazards. These zoning regulations are primarily directed toward height limits but also address light emissions to avoid potential hazards to aircraft resulting from illuminated signs and structures within airport hazard areas. The City of Los Angeles Municipal Code, Section 93.0117, regulates light spillover in residential areas.

Westchester-Playa Del Rey Community Plan

The Westchester-Playa Del Rey Community Plan (Community Plan) of the City of Los Angeles General Plan establishes goals, objectives, and programs that are intended to protect the scenic resources and vistas within the community planning area. The Community Plan includes policies addressing the preservation of visual resources in residential areas, along public streets, and facilities that enjoy designated scenic views. The Community Plan identifies the following visual resources: the Westchester Coastal Bluffs, the bluffs in Playa del Rey, Playa del Rey Beach, Dockweiler State Beach, Ballona Wetlands, Del Rey Lagoon and Park, and Ocean Front Walk and bike path. None of these resources is located in the immediate vicinity of the Project site. Dockweiler State Beach is approximately 0.5 mile west of the Project site. Chapter V of the Community Plan also includes design guidelines for the Community of Westchester's downtown, located directly to the east of Area 11. These guidelines include orienting buildings to main streets, designing for pedestrian-orientation, providing offset building massing and façade articulation, and landscape buffers adjacent to residential uses.

LAX Plan

The LAX Plan, an element of the City of Los Angeles General Plan, provides goals, objectives, policies, and programs that establish a framework for the development of facilities for movement and processing of passengers and cargo at LAX. It is intended to promote an arrangement of Airport uses that encourage and contribute to the modernization of the Airport in an orderly and flexible manner within the context of the City of Los Angeles and the Los Angeles region. Applicable aesthetics-related regulations of the LAX Plan are listed below:

- Goal 5: Acknowledge neighborhood context and promote compatibility between LAX and the surrounding neighborhoods:
 - Objective 5.01: Minimize negative impacts to surrounding residential land uses:
 - Land Use- Airport Northside Policy P1: Provide and maintain landscaped buffer areas along the northern boundary of Airport Airside and northern boundary of LAX Northside that include setbacks, landscaping, screening, or other appropriate mechanisms with the goal of avoiding land use conflicts, shielding lighting, enhancing privacy, and better screening views of airport facilities from adjacent residential areas.
 - Design Policy P2: Appropriately relate those airport facilities that are adjacent to community land uses to the scale and level of activity of those uses.
 - Design Policy P3: Relate Airport Landside facilities to the existing airport infrastructure in a clear, well-organized, functional, and compatible manner.
 - Design Policy P4: Update or integrate the following existing design plans into the LAX Conceptual Plan and/or Design Guidelines: LAX Street Frontage and Landscape Development Plan; LAX Air Cargo Facilities Design Guidelines; and LAX Northside Design Plan and Development Guidelines (1989).

- Design Policy P5: Develop and incorporate signage guidelines that provide guidance and establish controls for signage that are appropriate to an airport.

LAX Specific Plan

The LAX Specific Plan provides regulatory controls and incentives for the systematic and incremental execution of the LAX Plan, an element of the City of Los Angeles General Plan. The LAX Specific Plan specifies zoning and development regulations applicable to development at LAX, focusing primarily on land use, transportation, parking and signage regulations, with the land use regulations including not only comprehensive regulations but regulations specific to individual subareas of LAX (e.g., Airport Airside, Airport Landside, and LAX Northside). The LAX Specific Plan addresses setbacks, buffers, height limits, and landscaping within the airport area, particularly within the LAX Northside.

LAX Street Frontage and Landscape Development Plan Update

In 1994, LAWA adopted a Street Frontage and Landscape Development Plan as the integrated and coordinated landscape design guidelines for the perimeter areas of LAX, including the southern boundary along Imperial Highway; the eastern boundary, which includes Manchester Square, the Continental City site, and areas north and south of 111th Street west of Interstate 405; the northern boundary, which includes the Project site; and the western boundary of the Los Angeles Airport/El Segundo Dunes Preserve.

4.1.2.2 Existing Conditions

4.1.2.2.1 Visual Character

Aesthetics

Project Site

Overview

The discussion of existing visual character within the Project site is organized according to the specific scenic characteristics associated with each proposed district within the Project site: the LAX Northside Center District, LAX Northside Campus District, and the LAX Airport Support District (**Figure 2-4**).

The Project site is comprised of approximately 340 acres of primarily vacant, previously disturbed land that is surrounded by a highly-developed, urbanized mix of land uses. The Project site is located immediately south of the west City of Los Angeles coastal communities of Playa del Rey and Westchester. The western boundary of the Project site is located less than a half mile east of the Pacific Ocean, and less than two miles southeast of Marina del Rey. The Project site is situated south of the Westchester Bluffs and Loyola Marymount University (LMU), both notable visual features in the vicinity of the Project site.

The Project site topography is relatively flat east of Lincoln Boulevard. West of Lincoln Boulevard the Project site slopes downward from north to south, with an approximately 50-foot elevation drop at the steepest point on the Project site (roughly in the middle of Area 2). The Project site also slopes down towards Pershing Drive and the Pacific Ocean.

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The Project site primarily consists of vacant, previously disturbed land, with some existing facilities as described below.

LAX Northside Center District

The LAX Northside Center District is approximately 122 acres of relatively flat land and consists of both previously-disturbed vacant areas and areas with existing facilities and site improvements as shown in the photographs below.

Area 11

Area 11 is currently used as a temporary construction staging area for LAX projects (**Figure 4.1-1**). Current construction operations lend the Project site a disorderly form, as equipment is irregularly spaced throughout. The undeveloped nature of Area 11 is characterized by mostly exposed ground with ruderal/weedy vegetation and some trees. One major access road bisects the eastern portion of Area 11. The topography is primarily flat, although there are some mounds of dirt. Additionally, there is a cluster of rectangular shaped mobile trailers and storage units within central portion of the Project site. Area 11 is bounded by a chain-link fence with fabric and/or landscape that hides most of the equipment and activities at street level.

Figure 4.1-1 – Existing Visual Character of Area 11, Aerial View



Source: Google Earth, 2014

Area 12A East

Area 12A East is primarily vacant except for the existing City of Los Angeles Fire Department (LAFD) Fire Station Number 5 on the southwestern portion of this Area (**Figure 4.1-2**). A chain link fence borders the vacant portions of Area 12A East that abut La Tijera Boulevard and Westchester Parkway, while vacant portions of Area 12A East that abut West 88th Street and West 88th Place have landscaped soundwalls. Area 12A East was previously developed as part of a residential subdivision and the vacant portion of this Area retains abandoned paved areas which served as roads as well as lighting fixtures from its former residential use. Aside from abandoned paved streets and lighting fixtures, the vacant portion of Area 12A East is sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with 10 mature trees (defined as those having at least an 8-inch diameter at breast height).

[dbh]). The 10 mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, the vacant portion of Area 12A East does not contain known or designated valued visual resources.

Area 12A East also includes LAFD Station Number. 5, which was built in 2005 and is composed of two contemporary industrial-style buildings: a main building and a garage. The main building is two stories and the garage is one-story, and both have irregular-shaped plans. Both buildings have flat roofs covered with unknown materials and the main building features signal lights on the rooftop. The walls are clad with a combination of stucco, brick, and concrete block. Wall features include scoring in the stucco, decorative metal panels, and metal awnings. The main building has aluminum-framed windows with sills and hoods, arranged symmetrically. Fire Station Number 5 contains limited landscaping composed of groundcover and shrubs located mainly along Emerson Avenue and West 88th Place. A red brick soundwall and chain link fence are located north of Fire Station Number 5 between Emerson Avenue and Liberator Avenue. There are existing evergreen trees and shrubs between the soundwall and chain link fence. Given that there is no architectural distinction of Fire Station Number 5 and that it has no historic association (being built in 2005), the existing Fire Station Number 5 is not considered a valuable visual resource.

Figure 4.1-2 – Existing Visual Character of Area 12A East, Aerial View



Source: Google Earth, 2014

Area 12A West

Area 12A West is primarily vacant (**Figure 4.1-3**). A portion of Area 12A West has been and is being used as a temporary construction staging area. A chain link fence borders the portions of Area 12A West that abut Westchester Parkway, while portions of Area 12A West that abut the existing Westchester Golf Course are separated by vegetation and a dirt path. Area 12A West was previously developed as part of a residential subdivision and a portion of this Area retains abandoned paved areas which served as roads as well as lighting fixtures from its former residential use. Aside from abandoned paved streets and lighting fixtures, the vacant portion of

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Area 12A West is sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, Area 12A West does not contain known or designated valued visual resources.

Figure 4.1-3 – Existing Visual Character of Area 12A West, Aerial View



Source: Google Earth, 2014

Area 12B

The Westchester Golf Course located on Area 12B is characterized by large expanses of grass lawn. Trees are located throughout the golf course and a line of trees borders the golf course at the eastern edge between Manchester Avenue and West 88th Street and at the northern boundary along West 88th Street. A paved parking lot, one-story clubhouse, and sandy putting area are located on the northwest of the golf course. The clubhouse is a one-story beige-painted brick structure with a red tile pitched roof. The golf course is surrounded by a chain-link fence and higher netting on the east, west, and south to prevent golf balls from being hit off-site. The southwest corner of Area 12B also contains a white communications tower approximately two-stories high.

The Westchester Golf Course clubhouse is a Spanish Colonial Revival-style building with a north-facing orientation. It is one-story with a rectangular plan and two distinct ends: a western half that serves as the restaurant and pro shop, and an eastern half used to store and wash golf carts.

Area 13

Area 13 is split nearly evenly between vacant land and the existing First Flight Child Development Center (**Figure 4.1-4**). A chain link fence borders the vacant portions of Area 13 that abut Lincoln Boulevard as well as the existing Westchester Golf Course. Existing above ground electricity transmission lines also run along the eastern boundary of Area 13. An 8-foot-tall brick wall runs along the vacant portions of Area 13 that abut the existing First Flight Child

Development Center. The First Flight Child Development Center located on Area 13 is a multi-building campus laid out in a U-shape plan. The main building in the First Flight Child Development Center that is publicly visible is a one-story contemporary commercial building with an irregular plan. The walls are clad with a combination of stucco, concrete block, and concrete. A grey brick wall and landscaping with grass, flowering groundcover, and trees fronts Lincoln Boulevard.

A portion of Area 13 contains a parking lot with marked parking spaces and with security lighting. Aside from parking lot and lighting fixtures, the vacant portion of Area 13 is sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) but no mature trees (Refer to 4.3 Biological Resources and Appendix F). Therefore, the vacant portion of Area 13 does not contain known or designated valued visual resources.

Figure 4.1-4 – Existing Visual Character of Area 13, Aerial View



Source: Google Earth, 2014

LAX Northside Campus District

The LAX Northside Campus District is approximately 105 acres and primarily consists of previously-disturbed vacant land. The LAX Northside Campus district has sloping topography that generally decreases in elevation moving south towards Westchester Parkway and west towards Pershing Drive.

Area 1

Area 1 is primarily vacant except for the existing Jet Pets Animal Quarantine Facility located on the southwestern portion of this Area (**Figure 4.1-5**). A chain link fence borders the vacant portions of Area 1 that abut Westchester Parkway, South Pershing Drive, and Falmouth Avenue. Vacant portions of Area 1 that abut the existing residential developments on the north boundary have landscape and fencing separating the two uses. The vacant portion of Area 1 is sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) but no mature trees (Refer to 4.3 Biological Resources and Appendix F). Therefore, the vacant portion of Area 1 does not contain known or designated valued visual resources.

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Area 1 also includes the existing Jet Pets Animal Quarantine Facility, which began its current operations in 1973 in existing structures that were built starting in 1947. The main office has a rectangular plan and a south-facing orientation. The walls are constructed of painted concrete block. The main office building has a very low-pitch front-gabled and shed roof of composition shingle which features mounted lights and wooden signage. The boarding facility has a T-shaped plan and an east-facing orientation. The boarding facility walls are constructed of painted concrete block and the flat roof is covered with composition shingle. The associated office building has a rectangular plan and an east-facing orientation. The associated office building walls are constructed of painted concrete block and the flat roof is covered with composition shingle. Given that there is some association of the Jet Pets Animal Quarantine Facility with historical uses, it is potentially considered a valuable visual resource.

Figure 4.1-5 – Existing Visual Character of Area 1, Aerial View



Source: Google Earth, 2014

Area 2

Area 2 is primarily vacant (**Figure 4.1-6**). A chain link fence surrounds all of Area 2. Area 2 was previously developed as part of a residential subdivision and a portion of this Area retains abandoned paved areas which served as roads as well as lighting fixtures from its former residential use. Aside from abandoned paved streets and lighting fixtures, Area 2 is also sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, Area 2 does not contain known or designated valued visual resources. The majority of Area 2 along its northern edge is directly adjacent to the backyards of single-family residences and is separated from these uses by private walls or fences. An existing secured, paved alley also separates single-family residences from Area 2 between Rayford Drive and Loyola Boulevard.

Figure 4.1-6 – Existing Visual Character of Area 2, Aerial View

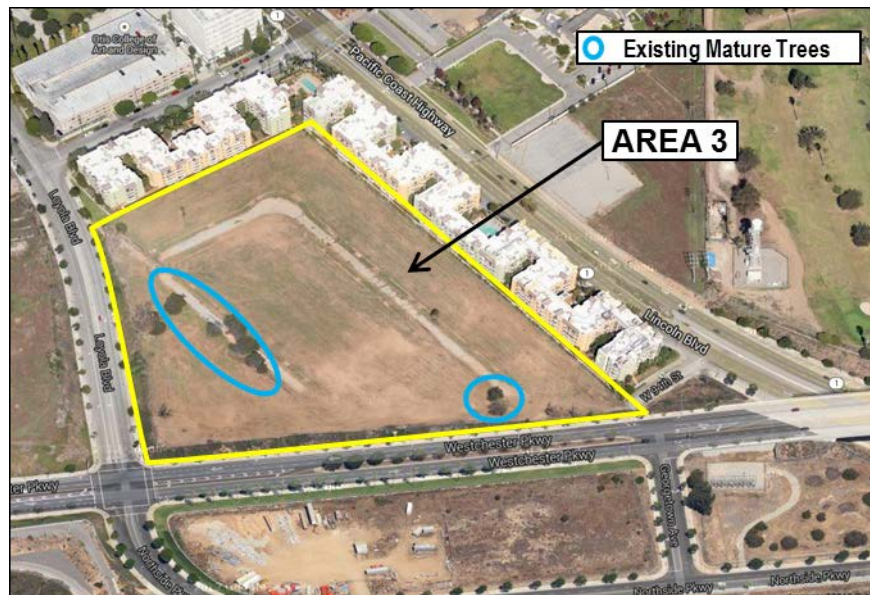


Source: Google Earth, 2014

Area 3

Area 3 is primarily vacant (**Figure 4.1-7**). A chain link fence surrounds all of Area 3. Area 3 was previously developed as part of a residential subdivision and a portion of this Area retains abandoned paved areas which served as roads as well as lighting fixtures from its former residential use. Aside from abandoned paved streets and lighting fixtures, Area 3 is also sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, Area 3 does not contain known or designated valued visual resources. Area 3 is separated from existing multi-family development to the north and east by a block wall.

Figure 4.1-7 – Existing Visual Character of Area 3, Aerial View



Source: Google Earth, 2014

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LAX Northside Airport Support District

The LAX Northside Airport Support District is approximately 110 acres and primarily consists of disturbed vacant land. The LAX Airport Support District contains low, groundcover landscaping along the length of Westchester Parkway. Westchester Parkway is the northern boundary of the LAX Northside Airport Support District and separates this district from the LAX Northside Campus District and LAX Northside Center District with four or more travel lanes, bike lanes, a striped and landscaped median, and left and right turn pockets. The western portion of Westchester Parkway offers the most distant views due to the fact that neighboring areas are vacant. The eastern portion of Westchester Parkway (east of its intersection with Loyola Boulevard) is located in a topographical depression, and surrounding hillsides on the north and south sides of Westchester Parkway obstruct views to the north toward the LAX Northside Campus District, and south toward the LAX Northside Airport Support District and LAX North Airfield. Visibility of the LAX Northside Airport Support District is limited along Westchester Parkway between Pershing Drive and Falmouth Avenue as Westchester Parkway is lower in elevation than Area 4 in this location. Views here consist of landscaping along the northern edge of Area 4. Moving west along Westchester Parkway, Area 4 becomes more visible where Westchester Parkway is more level in elevation with Area 4, followed by blocked views as elevations change and a landscaped berm is introduced before Loyola Boulevard. Views to the south from Westchester Parkway are partially screened between Loyola Boulevard and Colegio Drive by a chain link fence with green privacy fabric. The LAX Northside Airport Support District and LAX beyond is highly visible between Colegio Drive and Sepulveda Westway from Westchester Parkway where the topography is relatively flat.

Area 4

Area 4 is primarily vacant, with disturbed soils, and is also used as a temporary construction staging area for LAX projects (**Figure 4.1-8**). Current construction operations lend the Area a disorderly form as equipment is irregularly spaced throughout. The undeveloped nature of Area 4 is characterized by mostly exposed ground with ruderal/weedy vegetation. The topography is primarily flat, although there are some mounds of dirt. Additionally, there is a cluster of rectangular shaped mobile trailers and storage units within western portion of the Area 4. Area 4 is bounded by a chain-link fence.

Figure 4.1-8 – Existing Visual Character of Area 4, Aerial View



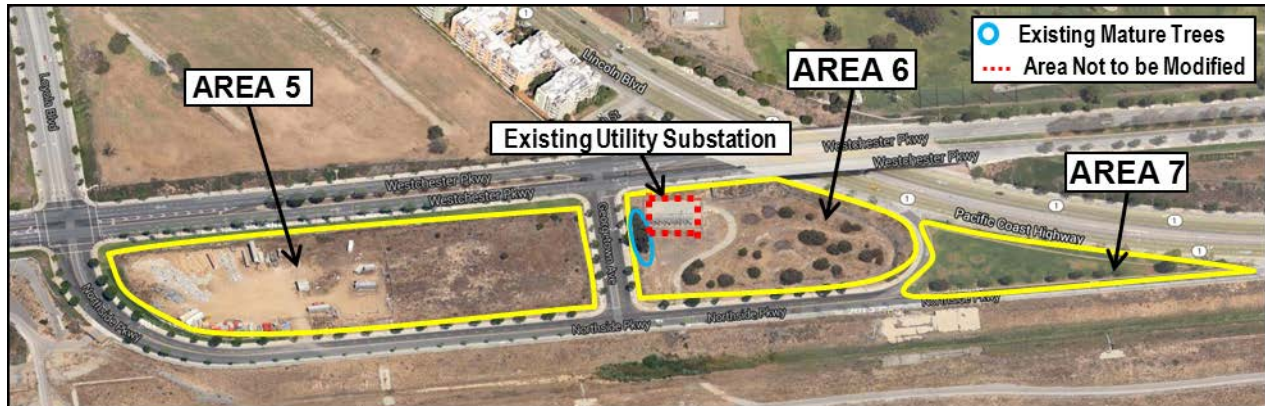
Source: Google Earth, 2014

Area 5

Area 5 is primarily vacant, with disturbed soils, and is also used as a temporary construction staging area for LAX projects (**Figure 4.1-9**). Current construction operations lend the Project site a disorderly form as equipment is irregularly spaced throughout. The undeveloped nature of

Area 5 is characterized by mostly exposed ground with ruderal/weedy vegetation. The topography is primarily flat, although there are some mounds of dirt. Additionally, there is a cluster of rectangular shaped mobile trailers and storage units within western portion of the Area 5. The entire Area 5 is bounded by a chain-link fence.

Figure 4.1-9 – Existing Visual Character of Areas 5, 6, and 7, Aerial View



Source: Google Earth, 2014.

Area 6

Area 6 is primarily vacant, with disturbed soils, but also contains an existing utilities substation (**Figure 4.1-9**). A chain link fence borders Area 6, which is also sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, the vacant portions of Area 6 do not contain known or designated valued visual resources.

The substation in Area 6 is small and the electrical equipment rests on a concrete pad. There is fencing surrounding the substation but no fabric or landscaping obstructs views into the substation. The topography is a sloping one from north to southeast. Electrical utilities substations are not considered valued visual resources. This use will not be modified as part of the proposed Project.

Area 7

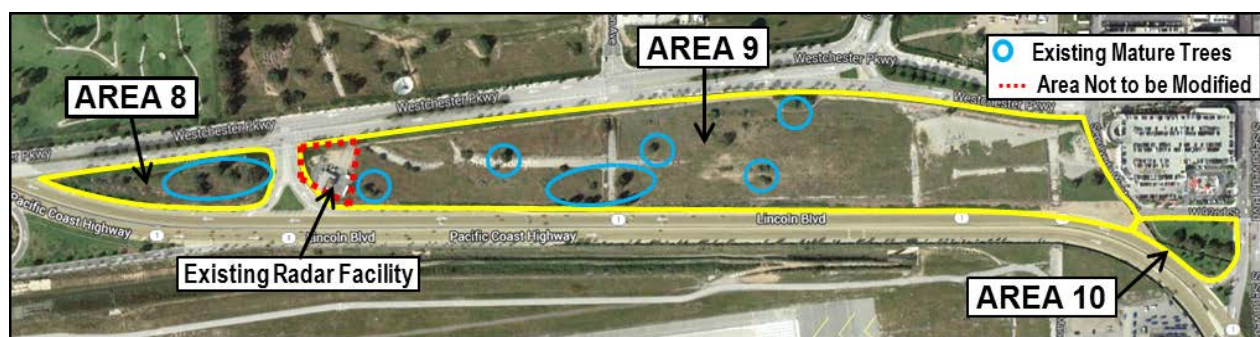
Area 7 is primarily vacant and landscaped (**Figure 4.1-9**). The topography is sloping, from north to southeast. Area 7 does not have restricted access and primarily serves as a landscaped area at the junction of two roads, Lincoln Boulevard and Northside Parkway. Area 7 does not have any known valued visual resources.

Area 8

Area 8 is primarily vacant (**Figure 4.1-10**). A chain link fence borders all of Area 8. Area 8 is sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, Area 8 does not contain known or designated valued visual resources.

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Figure 4.1-10– Existing Visual Character of Areas 8, 9, and 10, Aerial View



Source: Google Earth, 2014

Area 9

Area 9 includes vacant land and an existing FAA radar facility (**Figure 4.1-10**). A chain link fence borders all of Area 9. Area 9 was previously developed as part of a residential subdivision and a portion of this Area retains abandoned paved areas which served as roads as well as lighting fixtures from its former residential use. Aside from abandoned paved streets and lighting fixtures, Area 9 is also sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Tree Ordinance. Therefore, Area 9 does not contain known or designated valued visual resources.

The existing FAA radar facility in Area 9 is utilized at LAX, which was constructed between 1978 and 1982. It is composed of three small and one large one-story buildings, as well as a metal rotating structure. The large building is L-shaped and the other three buildings have a rectangular footprint. All four buildings feature flat roofs covered with unknown material. The walls are constructed of concrete block with some wall vents. All of the buildings lack windows, and their entries contain single and double metal doors with small concrete stoops. The radar is a metal rotating structure atop a metal scaffold structure. The radar facility is enclosed by a perimeter chain link fence and features a paved surface parking lot. This use will not be modified as part of the proposed Project.

Area 10

Area 10 is primarily vacant and landscaped (**Figure 4.1-10**). The topography is generally flat. Area 10 does not have restricted access and primarily serves as a landscaped area at the junction of two roads, Lincoln Boulevard and Sepulveda Boulevard. Area 10 does not have any known valued visual resources.

Project Site Vicinity

The following is a discussion of the general character of the areas that are located north, south, east, and west of the Project site. As described in Chapter 2 Project Description, the Project site is adjacent to the City of Los Angeles communities of Westchester and Playa del Rey to the north, east, and west and the LAX North Airfield to the south. The communities of Westchester and Playa del Rey are generally bounded by Centinela Avenue; La Brea Avenue; the City of Los Angeles boundaries with unincorporated County of Los Angeles; the City of Inglewood; the City of El Segundo; Dockweiler State Beach; Ballona Creek, Bay Street; and Jefferson Boulevard.

The communities of Westchester and Playa del Rey contain approximately 5,766 net acres of land. Several representative photo locations were selected to demonstrate the existing visual character of the Project site vicinity within the Community of Westchester, the Westchester Business District, the area along Westchester Parkway, and Playa del Rey, which are described below.

Community of Westchester

The Community of Westchester is located north and east of the Project site. The Community of Westchester is bounded roughly by Jefferson Boulevard and Centinela Boulevard to the north, the Project site to the south, the 405 Freeway to the east, and Playa del Rey to the west. Commercial districts and denser multi-family housing are located along Sepulveda Boulevard, Manchester Avenue, and Lincoln Avenue. Single family residential neighborhoods are located between Manchester Avenue, Lincoln Boulevard, and the Project site and between Manchester Avenue, Sepulveda Westway, and the Westchester Golf Course.

Residential Uses. The single-family neighborhoods within the Community of Westchester, located directly north of the Project site, were constructed as a planned residential subdivision during the 1940s and early 1950s. One-story single-family ranch style homes with pitched roofs, grass front lawns, detached garages and consistent lot sizes are characteristic of this area. The Community of Westchester continues to demonstrate a strong sense of visual continuity and cohesion as no major intrusive or discordant development has occurred since the time of its construction.¹

Photos taken at the intersection of West 86th Street and Kentwood Avenue (**Figure 4.1-11**) and at the intersection of West 83rd Street and Loyola Boulevard (**Figure 4.1-12**) in single-family residential subdivisions illustrate the visual character of the Community of Westchester's residential areas. The residential areas are characterized by single-story homes with mature landscaping. View 1 illustrates the typical residential style single-story homes with pitched roofs, grass landscaped lawns, and detached garages. Above-ground utility poles and taller palm trees are also visible in the background of these homes. View 2 depicts the character of the streetscape in the Community of Westchester's residential areas, which includes mature landscaping, paved sidewalks (some with landscaped parkways as depicted), street lights, and on-street parking.

Westchester Business District. The Westchester-Playa Del Rey Community Plan identifies the commercial district along either side of Sepulveda Boulevard, between Manchester Avenue and West 92nd Street as the Westchester Business District. The Westchester Business District is located along Sepulveda Boulevard with some commercial development also situated along Manchester Avenue, directly east of the Project site.² The Westchester Business District consists of a mix of moderate intensity commercial development including retail, shopping, dining, hotel and office. Development is characterized by mostly one, two, and three-story structures, with few significantly taller buildings. Both Sepulveda Boulevard and Manchester Avenue are major arterials that experience substantial vehicular traffic throughout the day. Commercial districts along Sepulveda Boulevard offer larger parking lots to the rear of the buildings. This brings the commercial development to the edge of the sidewalk on both sides of Sepulveda Boulevard, lending the street a "downtown/urban" character that is also of pedestrian

¹ City of Los Angeles, *Westchester-Playa Del Rey Community Plan*, 2004.

² Sepulveda Boulevard is located east of LAX Northside Center and Manchester Avenue is located north of both the LAX Northside Center and LAX Northside Campus.

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scale. Between Lincoln Boulevard and Sepulveda Boulevard, Manchester Avenue has a few commercial uses, but is largely comprised of three to four story multi-family housing units.

The two existing views that represent the Westchester Business District portray the existing visual character of the Westchester Business District. View 3 is located on the west side of Sepulveda Boulevard at Manchester Avenue and faces south towards LAX (**Figure 4.1-13**). View 3 demonstrates the commercial character of the Sepulveda Boulevard. Buildings in the foreground of View 3 range from one- to three-stories, while those in the distance (closer to LAX) are in the range of seven- to eight-stories tall. Building facades exhibit a variety of styles. Some include awnings and projected signage, while some do not have awnings and have wall-mounted or pole signs. Buildings are generally clad in stucco with earth-toned colors including brown, beige, and brick. The roadway is characterized by three lanes of paved and striped lanes in each direction and on-street parking. A center median is landscaped with shrubs and palm trees. Paved sidewalks exist on both sides of the streets with landscaping, street lights, bus shelters, and trash receptacles.

View 4 is located at the intersection of Sepulveda Boulevard and La Tijera Boulevard (**Figure 4.1-14**). View 4 faces west down La Tijera towards the Project site. The street wall of commercial buildings in this area is broken by large areas of parking. While commercial uses are visible in the foreground of View 4, these structures are set back from the street, with parking situated in front of the buildings. The roadway is characterized by two lanes of paved, stripe traffic in both direction and on-street parking. Landscaping includes trees planted along parkways and in parking lots. Light poles and above-ground utility poles are visible in the background. The sidewalk includes bus benches and trash receptacles.



OVERVIEW MAP



VIEW LOCATION MAP



Existing View 1
West 86th St. and Kentwood Ave.

Draft Environmental Impact Report
LAX Northside Plan Update

FIGURE
4.1-11

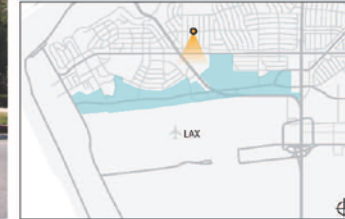
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OVERVIEW MAP



VIEW LOCATION MAP



**Existing View 2
West 83rd Street and Loyola Blvd.**

**Draft Environmental Impact Report
LAX Northside Plan Update**

**FIGURE
4.1-12**



OVERVIEW MAP



VIEW LOCATION MAP



**Existing View 3
Sepulveda Blvd. and Manchester Ave.**

**Draft Environmental Impact Report
LAX Northside Plan Update**

**FIGURE
4.1-13**

4.1 Aesthetics



**Existing View 4
La Tijera and Sepulveda Blvd.**

**Draft Environmental Impact Report
LAX Northside Plan Update**

**FIGURE
4.1-14**

Community of Playa del Rey

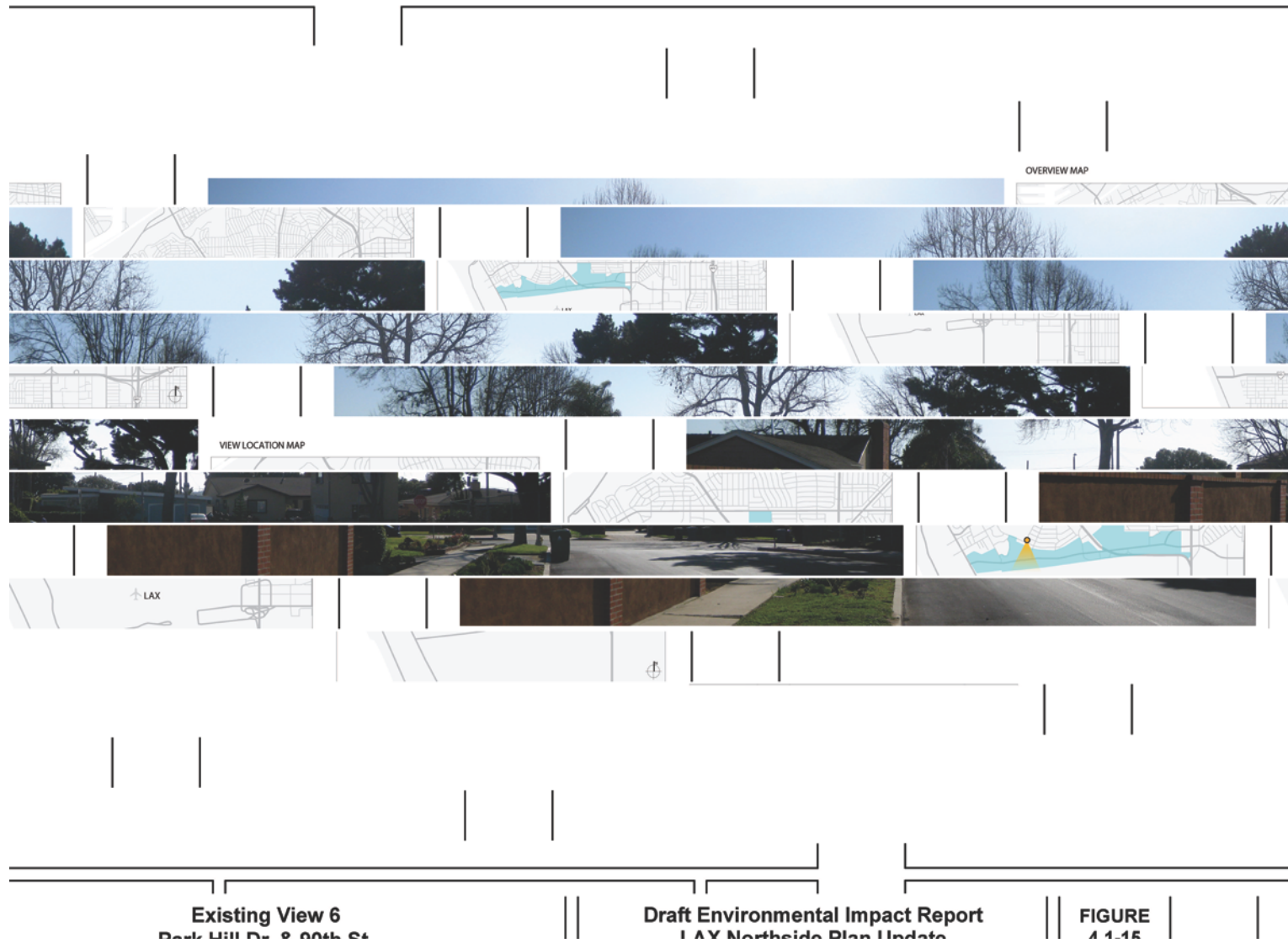
The residential community of Playa del Rey is bordered by Marina del Rey to the north; LAX-owned open space, LAX, and Dockweiler Beach to the south; the Community of Westchester to the east; and the Pacific Ocean to the west. The eastern portion of the residential community of Playa del Rey was constructed during the same time frame as the Community of Westchester and consists mainly of one- to two-story ranch style homes with pitched roofs and grass lawns on sloping terrain. Between Park Hill Drive and Falmouth Avenue and directly north of the Project site, Playa del Rey contains open space (baseball fields) and St. Bernard and Westchester High Schools. Westchester High School contains a complex of modern, rectangular white buildings, while St. Bernard High School is characterized by more traditional red brick structures. Further west, Playa del Rey contains more dense multi-family residential development up to four stories. Building styles vary but typically consist of light-colored structures with stucco walls and red tile or flat roofs. Many multi-story residential buildings also have balconies. As the community trends west closer to the Pacific Ocean, the relative density increases, and lot sizes become smaller reflecting its popularity, proximity, and accessibility to Dockweiler State Beach and the Pacific Ocean. Commercial areas of Playa del Rey are located along Culver Boulevard, Pershing Drive and Manchester Avenue. Commercial uses in Playa del Rey are low-density and characterized by smaller, individually-owned lots on a block face in a “village” style.

Views 6, 7, and 8 depict the existing visual character of the Playa del Rey residential land uses to the northwest of the Project site (west of Vista del Mar). View 6 is located on Park Hill Drive and 90th Street and faces south (**Figure 4.1-15**). View 6 consists of single-family homes. The architectural style of homes in this area varies, with some traditional ranch style homes with pitched roofs and detached garages, and one mid-century modern home with a flat roof and attached garage. Alterations to the original home design are also visible, such as the second-story addition and expansion of an original detached garage. The area includes mature street trees planted in landscaped parkways, as well as grass front lawns. Street lighting and utility poles are visible as well.

View 7 is located on Park Hill Drive at West 85th Street. View 7 faces south toward multi-family housing located along Manchester Avenue (**Figure 4.1-16**). The three-story, multi-family residence is clad in beige stucco with a flat roof. The streetscape includes one paved lane in each direction and on-street parking. The foreground includes a grass parkway with trees lined by private walls that consist of painted and unpainted blocks, while the background includes paved sidewalks in addition to a grass parkway. Ball fields associated with the St. Anastasia School are located just beyond Manchester Avenue. Single-story single-family homes and mature landscaping are seen in the distance.

View 8 is located at the intersection of Pershing Drive and Waterview Street (**Figure 4.1-17**). View 8 depicts a multi-family residential development located directly north of the Project site. The three-story structure has beige stucco siding with a brown shingle roof and red accents. Some units have balconies. Landscaping is limited to shrubs and street trees. The streetscape consists of two lanes of striped traffic in each direction. Utility poles and wires line the street. The raised, landscaped berm and Jet Pets are visible in the background.

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4.1 Aesthetics



OVERVIEW MAP



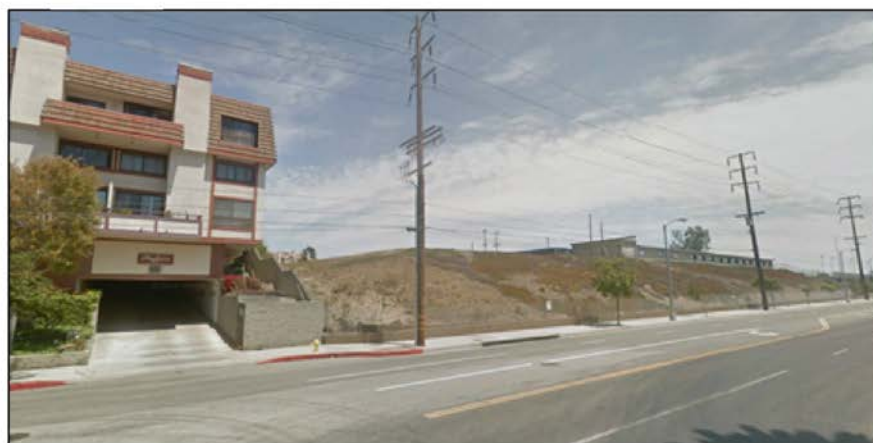
VIEW LOCATION MAP



Existing View 7
W. 85th St. and Park Hill Dr.

Draft Environmental Impact Report
LAX Northside Plan Update

FIGURE
4.1-16



OVERVIEW MAP



VIEW LOCATION MAP



**Existing View 8
Pershing Drive and Waterview Street**

**Draft Environmental Impact Report
LAX Northside Plan Update**

**FIGURE
4.1-17**

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LAX North Airfield

The LAX North Airfield is located directly south of the Project site. The LAX North Airfield includes paved runways, airport hangars, and other airport-related uses, including maintenance shops and warehouses. The LAX North Airfield is characterized by large expanses of paved runway, aircraft landing and taking off, and airport-related safety lighting.

View 9 portrays the existing visual character of the LAX North Airfield. View 9 is located at the intersection of Westchester Parkway and Georgetown Avenue, and faces south toward the LAX North Airfield (**Figure 4.1-18**). The existing character includes a chain link fence and grass vegetation in the foreground, while the industrial character associated with LAX is visible in the background. The LAX North Airfield buildings in this location are modern, grey concrete structures with no windows or ornamentation. Aircraft and terminal buildings are also visible. There is no landscaping on the LAX North Airfield, however there are street trees along the sidewalk that is directly north of the LAX security fence, and mature street trees in the foreground along Georgetown Avenue.

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4.1.2.2.2 Views

Project Site

For purposes of this analysis, “scenic resources” are considered special or particularly attractive and/or notable visual features that have been recognized and designated as such by applicable plans and regulations. The LAX Plan does not identify any scenic or view resources within the Project site.

Project Site Vicinity

The Project site vicinity includes the following scenic or view resources:

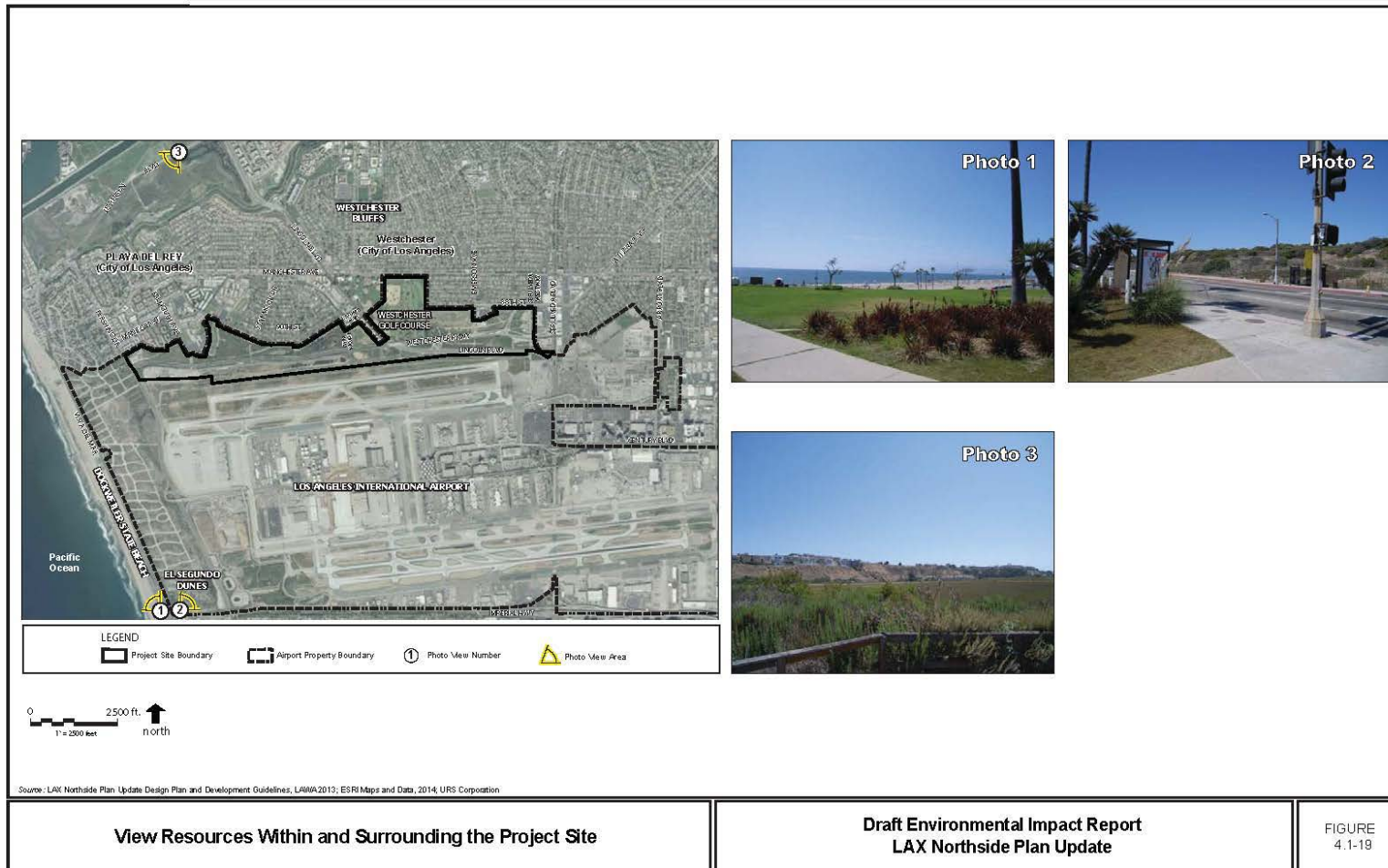
Dockweiler Beach State Park. The entrance to Dockweiler Beach State Park is located less than two miles southwest of the western boundary of the Project site. From its main entrance, Dockweiler Beach State Park extends north for more than a mile until it terminates at the entrance to the Marina del Rey harbor. Dockweiler Beach State Park encompasses approximately 125 acres of shore frontage along the Pacific Ocean.³ The Westchester-Playa Del Rey Community Plan identifies Dockweiler Beach State Park as a scenic resource (**Figure 4.1-19, Photo 1**). However, Dockweiler Beach State Park is not immediately adjacent nor accessible from the Project site. Views to the west of Dockweiler Beach State Park offer unobstructed vistas of the Pacific Ocean. Views to the east of Dockweiler Beach State Park look toward Vista del Mar highway and the Los Angeles Airport/El Segundo Dunes. The Los Angeles Airport/El Segundo Dunes are a vacant hilly area that is topographically diverse and rises sharply in elevation compared to Dockweiler Beach State Park.

Vista del Mar. Vista del Mar is a State-designated scenic highway. Vista del Mar runs north/south just east of Dockweiler Beach State Park. Western views from Vista del Mar offer unobstructed panoramas of the Pacific Ocean. Eastern views from Vista del Mar are confined due to the abrupt rise in elevation associated with the Los Angeles Airport/El Segundo Dunes Preserve (**Figure 4.1-19, Photo 2**).

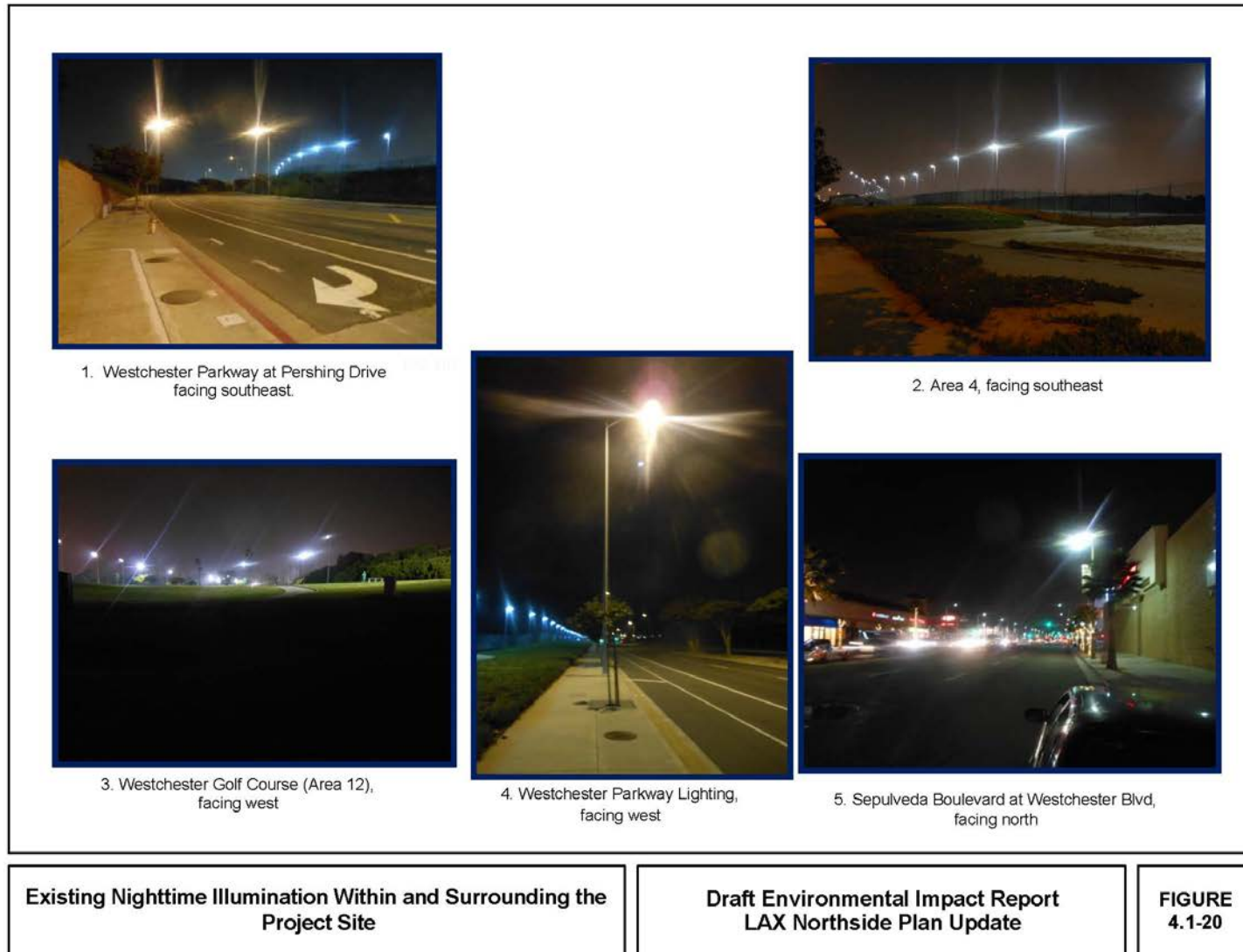
Westchester Bluffs. The Westchester Bluffs are identified as a locally scenic resource in the Westchester-Playa Del Rey Community Plan (**Figure 4.1-19, Photo 3**). The Westchester Bluffs are located less than a mile to north of the Project site. The Bluffs rise abruptly to an elevation of approximately 140 feet above mean sea level (AMSL).

³ City of Los Angeles, Westchester-Playa Del Rey Community Plan, 2004.

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4.1 Aesthetics



4.1.2.2.3 Light and Glare

Project Site

LAX Northside Center District

Area 11

Area 11 lighting includes perimeter street lights to the south and west along Westchester Parkway and La Tijera as well as lights associated with temporary construction bungalows and vehicles. Light-sensitive uses include residences located directly to the north across La Tijera. Residences are separated from Area 11 by private walls, La Tijera, landscaping along the south side of La Tijera, and the soundwall along the northern edge of Area 11.

Area 12A East

Area 12A East lighting includes perimeter street lights to the north, south, and east along 88th Place and 88th Street, Westchester Parkway and La Tijera and existing parking lot and exterior lighting for the LAFD Fire Station Number 5. Light-sensitive uses include residences located directly to the north across 88th Street and the Westchester-Emerson Adult School located directly to the north along 88th Place. Residences are separated from Area 12A East by private walls, 88th Street, landscaping along the south side of 88th Street, and the soundwall along the northern edge of Area 12A East.

Area 12A West

Area 12A West lighting includes street lighting along Westchester Parkway to the south. Area 12A West does not have adjacent light-sensitive uses as Westchester Parkway and airport support uses are located to the south, the heavily illuminated Westchester Golf Course is located to the west and north, and LAFD Station Number 5 and vacant Area 12A East are located to the east.

Area 12B

Area 12B lighting includes lighting associated with the existing Westchester Golf Course and clubhouse (**Figure 4.1-20, Photo 3**). Flood lights are present on the golf course, while the clubhouse and parking lot contain building and street lights.

Area 13

Area 13 lighting includes street lights along Lincoln Boulevard and parking lot, building, and driveway lighting for the existing First Flight Child Development Center. Existing lighting would remain in its existing location and configuration. Adjacent light-sensitive uses include multi-family residences to the south and west. These uses are separated from Area 13 by Lincoln Boulevard, which is an existing, well lit corridor.

LAX Northside Campus District

Area 1

Area 1 lighting includes street lights along Falmouth Avenue, Westchester Parkway, and Pershing Drive as well as exterior structure lights at the Jet Pets animal quarantine facility (**Figure 4.1-20, Photo 1**). Existing lighting associated with Jet Pets would remain in its existing location and configuration and construction activities would not take place on the portion of Area 1 that is currently occupied by Jet Pets. Adjacent light-sensitive uses include multi-family

4.1 Aesthetics

residences to the north and open space to the west. The Saint Bernard High School football field and parking lots located to the east contain flood lights. The football field and parking lots are not light-sensitive uses because they already use nighttime lighting and do not require minimal illumination for proper function, physical comfort, or commerce.

Area 2

Area 2 lighting includes street lights along Falmouth Avenue, West 92nd Street, Cum Laude Avenue, Westchester Parkway, and Loyola Boulevard. Adjacent light-sensitive uses include single-family residences to the north and multi-family residences to the east. The Saint Bernard High School baseball field and parking lots located to the west contain flood lights and are not light-sensitive uses because they already use nighttime lighting and do not require minimal illumination for proper function, physical comfort, or commerce. The Otis College parking structure to the east is not a light-sensitive use as it also contains illumination and does not require minimal illumination for proper function.

Area 3

Area 3 lighting includes street lights along Loyola Boulevard and Westchester Parkway. Adjacent light-sensitive uses include multi-family residences to the north and east. Building construction is required to be set back 20 feet from the north and east edges of the Area.

LAX Airport Support District

Area 4

Area 4 is substantially illuminated. Lighting on Area 4 includes street lighting along Pershing Drive and Westchester Parkway, perimeter lighting around existing airport support and construction laydown areas, and parking lot lighting (**Figure 4.1-20, Photo 2 and 4**). Open space located to the west across Pershing Drive is the only adjacent light-sensitive use.

Area 5

Area 5 lighting includes street lights along Westchester Parkway, Northside Parkway, and Georgetown Avenue. There are no light-sensitive uses adjacent to Area 5.

Area 6

Area 6 lighting includes street lights along Westchester Parkway, Northside Parkway, and Georgetown Avenue. There are no light-sensitive uses adjacent to Area 6.

Area 7

Area 7 lighting includes street lights along Lincoln Boulevard and Northside Parkway. There are no light-sensitive uses adjacent to Area 7.

Area 8

Area 8 lighting includes street lights along Lincoln Boulevard and Westchester Parkway. There are no light-sensitive uses adjacent to Area 8.

Area 9

Area 9 lighting includes street lights along Westchester Parkway, Lincoln Boulevard, and Sepulveda Westway and structure lighting for the LAX radar facility. There are no light-sensitive uses adjacent to Area 9. The adjacent commercial parking structure located to the east along Sepulveda Westway is not a light-sensitive use as it is already substantially illuminated and does not require minimal lighting for proper function, physical comfort, or commerce.

Area 10

Area 10 lighting includes street lighting along Lincoln Boulevard, West 92nd Street, and Sepulveda Boulevard.

Project Site Vicinity

The Project site vicinity includes an urbanized mix of commercial, airport-related, and residential uses which are sources of light. Operations at LAX run around the clock and, consequently, there is substantial nighttime illumination at the airport (**Figure 4.1-21**). In addition, Westchester Parkway has street lighting along the entire length of the Project site. The Westchester Business District includes street lighting and illuminated signage, which also contribute substantial amounts of nighttime illumination (**Figure 4.1-20, Photo 5**). The residential neighborhoods to the north of the Project site contain illumination typical of residential development along the streets and at residential structure.

Figure 4.1-21 – LAX Nighttime Lighting



Source: http://asa100.com/#/night-vision/new_things071,
<http://www.manick.com/Gallery/LAXTerminalNight09w.jpg>

In terms of glare, residential structures to the north and the commercial development to the east along Sepulveda Boulevard do not contain substantial reflective glass facades or surfaces (**Figure 4.1-22**). There are no existing sources of glare adjacent to the Project site.

Figure 4.1-22 – Existing Sources of Glare in Project Site Vicinity



Source: URS, 2014

4.1 Aesthetics

4.1.2.2.4 Shading

Project Site

The majority of the Project site is vacant and does not contain structures that would cast shadows. Additionally, there are no solar sensitive uses on the Project site.

LAX Northside Center District

Area 11

Area 11 does not contain any permanent structures that cast shadows. Construction bungalows and materials are low in height and do not cast off-site shadows. Shade-sensitive uses include outdoor spaces for residences located directly to the north across La Tijera Boulevard. Residences are separated from Area 11 by private walls, La Tijera Boulevard, landscaping along the south side of La Tijera Boulevard, and the soundwall along the northern edge of Area 11.

Area 12A East

The undeveloped portion of Area 12A East does not contain any structures that cast shadows. Area 12A East contains several mature trees, but in general, these trees do not cast shadows off-site. A few of these trees are located at the boundary of the vacant portion of Area 12A East and the sidewalk and they do cast shadows on the sidewalk and into the street, but not into sensitive areas.

Area 12A East contains the LAFD Fire Station Number 5 which is a two-story structure approximately 40 feet tall. The LAFD Fire Station Number 5 is located near residences in Area 12A East at the corner of Westchester Parkway and Emerson Avenue; however, its height is not enough to produce shadows that reach these residences (**Figure 4.1-23**). Shade-sensitive uses include outdoor spaces for residences located directly to the north across La Tijera Boulevard. Residences are separated from Area 12A East by private walls, La Tijera Boulevard, landscaping along the south side of La Tijera Boulevard, and the soundwall along the northern edge of Area 12A East.

Figure 4.1-23 – Existing Shadows on Area 12A East



Source: Rios Clementi Hale Studios, 2014

Area 12A West

Area 12A West does not contain any permanent structures that cast shadows. Shade-sensitive uses include the outdoor recreation space of the Westchester Golf Course located directly to the north.

Area 12B

Area 12B contains the one-story Westchester Golf Course clubhouse. Shade-sensitive uses include the outdoor spaces of residences located directly to the north along West 88th Street and residences located directly to the north across Manchester Avenue. The proposed Project would not introduce any new structures to Area 12B that would cast shadows.

Area 13

Area 13 contains the existing one-story First Flight Child Development Center. The undeveloped portion of Area 13 does not contain permanent structures that cast shadows. Shade-sensitive uses include the outdoor recreation space of the Westchester Golf Course located directly to the north and east.

LAX Northside Campus District

Area 1

Area 1 contains the Jet Pets Animal Quarantine Facility which consists of three single-story buildings that cast shadows. However, they are located far from the nearest sensitive uses (residences at the northern boundary of Area 1), and do not cast shadows off-site.

Area 2

Area 2 does not contain any significant structures that cast shadows. Area 2 was previously developed as part of a residential subdivision and a portion of this Area retains abandoned lighting fixtures which can cast shadows. However, these abandoned light fixtures are all located internally within Area 2, and do not cast shadows on residences to the north of Area 2.

4.1 Aesthetics

Area 2 has a large number of mature trees, and similar to the lighting fixtures, these trees are all located internally within Area 2 and do not cast shadows off-site. Shade-sensitive uses in the vicinity of Area 2 include backyards of residences to the north and a school playground to the west.

Area 3

Area 3 does not contain any significant structures that cast shadows. Area 3 was previously developed as part of a residential subdivision and a portion of this Area retains abandoned lighting fixtures which can cast shadows. However, these abandoned light fixtures are all located internally within Area 3, and do not cast shadows on residences to the north and east of Area 3. Area 3 has several mature trees, and similar to the lighting fixtures, these trees are all located internally within Area 3 and do not cast shadows off-site. Shade-sensitive uses in the vicinity of Area 3 include balconies and common open space of the multi-family residences to the east and north.

LAX Airport Support District

Area 4

Area 4 does not contain any permanent structures that cast shadows. Construction bungalows and materials are low in height and do not cast off-site shadows. There are no shade-sensitive uses in the vicinity of Area 4.

Area 5

Area 5 does not contain any permanent structures that cast shadows. Construction bungalows and materials are low in height and do not cast off-site shadows. There are no shade-sensitive uses in the vicinity of Area 5.

Area 6

Area 6 contains only one permanent structure, the existing electric utility substation. The existing electric substation has components that are low in height and do not cast off-site shadows. Area 6 has several mature trees that are all located along Georgetown Avenue which cast shadows on the sidewalk and the street. However, there are no shade-sensitive uses in the vicinity of Area 6.

Area 7

Area 7 does not contain any permanent structures that cast shadows. There are no shade-sensitive uses in the vicinity of Area 7.

Area 8

Area 8 does not contain any permanent structures that cast shadows. Area 8 has several mature trees that are all located internally within Area 8 and do not cast shadows off-site. There are no shade-sensitive uses in the vicinity of Area 8.

Area 9

Area 9 contains only one permanent structure, the existing FAA radar facility on the western end. The existing FAA radar facility contains a one-story structure with the antenna mounted on top which casts off-site shadows. However, these shadows are cast on sidewalks and nearby streets. Area 9 has several mature trees that are primarily located internally within Area 9. Any of the mature trees located at the periphery of Area 9 cast shadows on sidewalks and streets. There are no shade-sensitive uses in the vicinity of Area 9.

Area 10

Area 10 does not contain any permanent structures that cast shadows. There are no shade-sensitive uses in the vicinity of Area 10.

Project Site Vicinity

The Project site vicinity includes larger structures that cast shadows. Taller structures approximately four stories high exist along Sepulveda Boulevard. However, most of these are located in commercial corridors and do not affect shade-sensitive uses.

4.1.3 Impact Analysis

4.1.3.1 Methodology

The following subsections outline the methodology used to assess the aesthetics impacts associated with the proposed Project.

4.1.3.1.1 Visual Character

The City of Los Angeles CEQA Thresholds Guide identifies seven thresholds or review standards that are used to evaluate the significance or degree of impact a project may have on the visual environment. For purposes of this analysis the review standards were broken into thresholds applicable for the evaluation of “Aesthetics,” and thresholds applicable for the review of “Views.” The first two factors refer to changes in the character of an area for its existing conditions and have been combined into the first significance threshold that addresses impacts on valued resources. The next four factors are all contributors to the relationship between new development (i.e., project design/site integration, image, density, height, bulk, setbacks, signage, etc.) and existing development. These factors have been combined in the second significance threshold that addresses impacts on the visual character of the surrounding area. The seventh factor has been incorporated directly into the third significance threshold that addresses impacts regarding the regulatory setting

Aesthetics

The aesthetics analysis is based on a comparison of the existing aesthetic and visual quality within and surrounding the Project site. Potential future aesthetics and view analyses are based on the proposed Project, including the Project Design Features. For purposes of this analysis, the maximum potential building envelopes are assumed. The analysis of aesthetics is based on a three-step process and outlined below:

- Step 1: A description of massing and general configuration of buildings, open space, and proposed landscaping treatments around the Project site edges is provided. This description is based on the directives and policy guidelines included in the Project Design Features.
- Step 2: The resulting predicted appearance of the Project site under the proposed Project is compared to the current visual conditions and character of the Project site. A determination of whether and/or to what extent the visual character could be degraded as a result of development scenarios that would be allowed under the proposed Project is described.
- Step 3: A comparison of the anticipated appearance of development to standards within the existing plans and policies which are applicable to the Project site (regulatory analysis).

4.1 Aesthetics

Views

View locations were selected within the Community of Westchester residential land uses, the Westchester Business District, along Westchester Parkway, and within Playa del Rey residential land uses, and are representative of the existing views from the Project site vicinity. The analysis is based on potential build-out conditions that would be allowed under the proposed Project, including factors such as height limits and setback standards included as part of the proposed Project. Impacts are evaluated based on the extent to which existing views would be altered, and whether a line of sight toward a particular view would be obstructed. Potential alterations resulting from development within the Project site are compared to the existing view conditions. The analysis considers whether new Project Design Features could enhance⁴ or detract from viewing conditions as result of build-out scenarios. Specifically, potential significant impacts to existing views were determined through the following evaluation process:

1. Potential obstructions of existing view resources were determined through the preparation of simulations showing the build-out conditions (according to maximum height and minimum setback provisions) allowed under the proposed Project. It was assumed that any obstruction of a view resource resulting from development allowed under the proposed Project would constitute an adverse impact.
2. Obstructions produced as a result of development allowed under the proposed Project, as visible from specific viewpoints were evaluated. It was determined that a significant impact would occur where an obstruction would: 1) obstruct views of a valued resource; 2) cover more than an incidental/small portion of the resource; and 3) occur along a public view area.
3. An assessment of whether the proposed Project contains design features to mitigate the potential impact associated with particular view obstructions. Mitigating factors include a proposed design feature that would lessen the impact of the proposed Project as experienced from a particular view point.

4.1.3.1.2 Light and Glare

Light and glare also influence the existing visual character of an area. "Lighting" is defined as artificial light emissions, or the degree of brightness, generated by a given source. The Illuminating Engineering Society of North America (IES) defines glare as "the sensation produced by luminance in the visual field that is sufficiently greater than the luminance to which the eye has adapted to cause annoyance, discomfort, or loss of visual performance and visibility."⁵

Reflective light or glare is primarily a daytime phenomenon caused by the reflection of sunlight or artificial light by highly polished surfaces such as window glass or reflective materials, and to a lesser degree from broad expanses of light-colored surfaces. Reflective light is common in urban areas, where it can be an annoyance for residents and pedestrians and can create hazards for motorists.

4.1.3.1.3 Shading

The analysis of potential shading impacts is based on the maximum potential height of buildings associated with the proposed Project, the length of the shadow that would be cast by these

⁴ Enhancements are considered where the creation of new resources or new view locations may be created from a viewpoint.

⁵ IES, 1993.

potential buildings, and the relation of the shadow to sensitive uses. To show this, shading diagrams were produced to illustrate the shadow patterns associated with the proposed allowable development under the proposed Project. Because no specific buildings are currently proposed for the Project site, the shading analysis evaluates the shading effects from building envelopes defined by the maximum height and minimum setback proposed on an individual area. This produces a shadow effect that is equal to the greatest shadow impact that could occur under the proposed Project. However, the proposed Project includes additional limitations and guidelines on the amount and character of the development, particularly where it is adjacent to sensitive uses. This shading analysis using building envelopes results in a conservative analysis.

4.1.3.2 Significance Thresholds

The discussion of significance is based on the City of Los Angeles CEQA Thresholds Guide. Significance thresholds are divided into categories applicable to “Aesthetics/Visual Quality,” “Scenic Resources and Scenic Vistas,” “Light and Glare,” and “Shading” as described in the methodology section above.

4.1.3.2.1 Visual Character

Aesthetics

Significance determinations for impacts pertaining to visual character are made considering the following factors:

- The amount or relative proportion of existing features or elements that substantially contribute to the valued visual character or image of a neighborhood, community, or localized area, which would be removed, altered, or demolished;
- The amount of natural open space to be graded or developed;
- The degree to which proposed structures in natural open space areas would be effectively integrated into the aesthetics of the site, through appropriate design, etc.;
- The degree of contrast between proposed features and existing features that represent the area’s valued aesthetic image;
- The degree to which a proposed zone change would result in a building that would detract from the existing style or image of the area due to density, height, bulk, setbacks, signage or other physical elements;
- The degree to which the project would contribute to the area’s aesthetic value; and
- Applicable guidelines and regulations.

Based on these factors, a significant impact on aesthetics would occur if the direct and indirect changes in the environment that may be caused by the proposed Project result in the following future conditions:

- Adoption of the proposed Project would substantially alter, degrade, or eliminate the existing visual character of the area, including valued existing features, natural open space or other valued resources;
- Adoption of the proposed Project would allow development that would substantially contrast with the visual character of the surrounding area and its valued aesthetic image; or

4.1 Aesthetics

- Adoption of the proposed Project would preclude attainment of existing aesthetics regulations.

Views

According to the City of Los Angeles CEQA Thresholds Guide, the determination of significance related to obstruction of views shall be made on a case-by-case basis, considering the following factors:

- The nature and quality of recognized or valued views (such as natural topography, settings, man-made or natural features of visual interest, and resources such as mountains or the ocean);
- Whether the project affects views from a designated scenic highway, corridor, or parkway;
- The extent of obstruction (e.g., total blockage, partial interruption, or minor diminishment); and
- The extent to which the project affects recognized views available from a length of a public roadway, bike path, or trail, as opposed to a single, fixed vantage point.

Based on these factors, a significant impact on views would occur if the direct and indirect changes in the environment that may be caused by the proposed Project result in the following future condition:

- Adoption of the proposed Project would substantially obstruct an existing view of a valued view resource from a prominent view location.

4.1.3.2.2 Light and Glare

According to the City of Los Angeles CEQA Thresholds Guide, the determination of significance related to lighting shall be made on a case-by-case basis, considering the following factors:

- The change in ambient illumination levels as a result of project sources; and
- The extent to which project lighting would spill off the project site and affect adjacent light-sensitive areas.

4.1.3.2.3 Shading

According to the City of Los Angeles CEQA Thresholds Guide, the determination of significance related to shading shall be made on a case-by-case basis, considering the following factor:

- A project impact would normally be considered significant if shadow-sensitive uses would be shaded by project-related structures for more than three hours between the hours of 9:00 a.m. and 3:00 p.m. Pacific Standard Time (between late October and early April), or for more than four hours between the hours of 9:00 a.m. and 5:00 p.m. Pacific Daylight Time (between early April and late October).

4.1.3.3 LAX Master Plan Commitments and Project Design Features

4.1.3.3.1 LAX Master Plan EIS/EIR Commitments

As part of the LAX Master Plan, LAWA adopted several mitigation measures and commitments pertaining to aesthetics to avoid or reduce environmental impacts. Since the Project site is located within the LAX Master Plan boundaries, LAWA is required to and intends to fulfill the commitments it has made in the LAX Master Plan for the proposed Project. The following commitments are applicable to the proposed Project and were considered in the aesthetics analysis herein.

- **Biotic Communities MM-(BC)-3: Conservation of Floral Resources – Mature Tree Replacement.** LAWA or its designee shall prepare and implement a plan to compensate at a ratio of 2:1 for the loss of approximately 300 mature trees, which would occur as a result of implementation of the LAX Northside project. The plan shall include provisions to census and map all mature trees with a diameter of at least 8 inches at breast height, which may be removed due to implementation of the LAX Northside project. This information shall be gathered prior to initiation of construction. The plan shall include a program by which replacement (at a ratio of 2:1) of all impacted mature trees shall be included in plans prepared for landscape treatments within the Master Plan boundaries, which would then be implemented by LAWA. The species of newly planted replacement trees shall be local native tree species to the extent feasible. Each mitigation tree shall be at least a 15-gallon or larger specimen.
- **Design, Art, and Architecture Applications/Aesthetics (DA)-1: Provide and Maintain Airport Buffer Areas.** Along the northerly and southerly boundary areas of the airport, LAWA will provide and maintain landscaped buffer areas that will include setbacks, landscaping, screening or other appropriate view-sensitive improvements with the goals of avoiding land use conflicts, shielding lighting, enhancing privacy and better screening views of airport facilities from adjacent residential uses. Use of existing facilities in buffer areas may continue as required until LAWA can develop alternative facilities.
- **DA-2: Update and Integrate Design Plans and Guidelines.** The following plans and guidelines will be individually updated or integrated into a comprehensive set of design-related guidelines and plans; LAX Street Frontage and Landscape Development Plan (June 1994), LAX Air Cargo Facilities Development Guidelines (April 1998; updated August 2002), and LAX Northside Design Plan and Development Guidelines (1989), including conditions addressing heights, setbacks and landscaping. The update will serve as a basis for reviewing future public and private development projects at LAX. The update will incorporate key provisions in current plans with an equivalent or greater level of compatibility and visual quality supported between LAX and adjacent land uses.
- **Mitigation Measure (MM)-DA-1: Construction Fencing.** Construction fencing and pedestrian canopies shall be installed by LAWA to the degree feasible to ensure maximum screening of areas under construction along major public approach and perimeter roadways, including Sepulveda Boulevard, Century Boulevard, Westchester Parkway, Pershing Drive, and Imperial Highway west of Sepulveda Boulevard. Along Century Boulevard, Sepulveda Boulevard, and in other areas where the quality of public views are a high priority, provisions shall be made by LAWA for treatment of the fencing to reduce temporary visual impacts.

4.1 Aesthetics

- **Light Emissions (LI)-2: Use of Non-Glare Generating Building Materials.** Prior to approval of final plans, LAWA will ensure that proposed LAX facilities will be constructed to maximize use of non-reflective materials and minimize use of undifferentiated expanses of glass.
- **LI-3: Lighting Controls.** Prior to final approval of plans for new lighting, LAWA will conduct reviews of lighting type and placement to ensure that lighting will not interfere with aeronautical lights or otherwise impair Airport Traffic Control Tower or pilot operations. Plan reviews will also ensure, where feasible, that lighting is shielded and focused to avoid glare or unnecessary light spillover. In addition, LAWA or its designee will undertake consultation in selection of appropriate lighting type and placement, where feasible, to ensure that new lights or changes in lighting will not have an adverse effect on the natural behavior of sensitive flora and fauna within the Habitat Restoration Area.

4.1.3.3.2 Project Design Features

Overview

The Project site is proposed to be developed according to design standards included in the proposed LAX Northside Design Guidelines and Standards. Project Design Features (PDFs) related to lighting, signage, landscaping, grading, height limits, setbacks, stepbacks, building locations, and buffers will determine how the proposed Project will be developed. Project Design Features applicable to the entire Project site are described here, followed by Project Design Features applicable to specific Districts and Areas.

Lighting and Reflective Materials

- **PDF Aesthetics (A)-1:** Lighting shall be designed to provide ambiance, safety, and security without unnecessary spillover or glare onto adjacent properties.
- **PDF A-2:** Indirect wall lighting or “wall washing” and overhead down lighting may be used to help reduce light trespass into adjacent properties.
- **PDF A-3:** Spotlighting or glare from any site lighting shall be shielded from adjacent properties and directed at a specific object or target area.
- **PDF A-4:** Exposed bulbs shall not be used.
- **PDF A-5:** Building light fixtures shall be designed or selected to be architecturally compatible with the main structure.
- **PDF A-6:** Lighting mounted above ten (10) feet from finish grade shall incorporate a full cut-off shield fixture.
- **PDF A-7:** When security lighting is necessary, it shall be recessed, hooded, and located to illuminate only the intended area.
- **PDF A-8:** Off-site glare and light trespass shall be prevented.
- **PDF A-9:** Glare or light trespass is prohibited on any adjacent streets, or within any adjacent properties.
- **PDF A-10:** Service area lighting shall be contained within the service yard boundaries and enclosure walls.

- **PDF A-11:** No light spillover shall occur outside the service area.

Walls and Fences

- **PDF A-12:** Fences and walls not associated to Recreation or Buffer Areas shall have a maximum height of eight (8) feet measured from the finished grade.
- **PDF A-13:** A six (6) foot planting strip shall be located adjacent to walls and fences and shall include shrubs, vines and ground cover identified in Chapter 7 of the proposed LAX Northside Design Guidelines and Standards.
- **PDF A-14:** Solid fences or walls shall be designed with both sides articulated with similar or complementary materials and colors as the primary buildings on site.
- **PDF A-15:** Chain link fencing (with or without slats), corrugated metal, and barbed/razor wire is prohibited within the Northside Center and Campus Districts.
- **PDF A-16:** Long expanses of walls (50 feet or greater) shall be broken up with projections or recessed elements, landscape pockets or changes in materials.
- **PDF A-17:** Where a wall or fence is located adjacent to a public right-of-way, a minimum six (6) feet landscaped setback shall be provided.

Building Form

- **PDF A-18:** Buildings within 150 feet of residences shall be located to maximize privacy through building orientation or off-setting windows on any walls facing a residence to prevent direct views into any neighboring windows.
- **PDF A-19:** Clerestory windows, translucent glass, and/or vision glass beginning in elevation for the second story or higher at a minimum of four (4) feet from finish floor is required to prevent direct sight lines into neighbors' windows and livable outdoor spaces.
- **PDF A-20:** No façade is allowed to be longer than 80 feet without four foot horizontal offset.
- **PDF A-21:** The visual mass of all buildings is required to be reduced by varying parapet or the roof by a minimum of two feet for every 40 feet of façade and varying façade material.
- **PDF A-22:** No material, including glass, may cover more than 35% of the façade.
- **PDF A-23:** Mirror or reflective surfaces are prohibited as primary building materials.
- **PDF A-24:** Building heights and locations are restricted to preserve views of visual resources to the maximum extent feasible.

Roofs

- **PDF A-25:** Roof parapets are required to be an integral part of building design, and not add-on elements.
- **PDF A-26:** Roofs are required to be light colored and encouraged to be green roofs.
- **PDF A-27:** Exterior roof ladders are prohibited. Roof-mounted equipment is required to be screened.

4.1 Aesthetics

Parking Structures

- **PDF A-28:** Parking structures are required to be designed to minimize visual impact from public view and residential areas through architectural articulation and additional accents at circulation points.
- **PDF A-29:** Lighting is required to be shielded so that the source of lighting is not visible at the property line.
- **PDF A-30:** A ten foot landscape island is required around any parking structure.
- **PDF A-31:** The parking lot illumination level shall achieve a uniformity ratio of 3 to 1 (average to minimum) with a maintained average of 1 foot candle and minimum of 0.3 foot candle.
- **PDF A-32:** Parking areas are required to be landscaped with one tree per every four parking spaces.
- **PDF A-33:** All areas not used for parking, loading, or pedestrian connectivity are also required to be landscaped.

Auxiliary Buildings and Parking Structures

- **PDF A-34:** Auxiliary buildings are not allowed along Westchester Parkway, Sepulveda Westway, La Tijera Boulevard, Loyola Boulevard, Falmouth Avenue, or Pershing Drive.
- **PDF A-35:** Parking structures are not allowed to front Westchester Parkway.

Signage

- **PDF A-36:** Signs are limited to a maximum of two signs on two elevations and may not project above the top of buildings.
- **PDF A-37:** Signs are prohibited from being visible from residential areas and shall be located on building frontages.
- **PDF A-38:** Signs can be internally illuminated only to a maximum of 2 foot candles above ambient levels.
- **PDF A-39:** Exposed light sources (neon or incandescent) are prohibited.
- **PDF A-40:** Signs shall not overlap architectural features on a building.
- **PDF A-41:** Tenant signs are not allowed to project above buildings in the manner of billboards.
- **PDF A-42:** Signs employing animated components, moving/flashing or blinking lights, exposed raceways, exposed ballast boxes or transformers, unedged or uncapped plastic letters or letters with no returns and exposed fastenings, luminous-vacuum formed type plastic letters, sandblasted wood type construction are prohibited.

Landscaping and Paseo

- **PDF A-43:** Landscape design would put an emphasis on enhanced streetscapes and pedestrian experiences and safety.

- **PDF A-44:** The palette will primarily be evergreen and native, allowing a consistent visual appeal year round, in addition to being drought-tolerant and non-invasive.

The proposed Project would also introduce a pedestrian-accessible paseo along the entire northern side of Westchester Parkway.

- **PDF A-45:** The paseo will be composed of a 12 foot wide decomposed granite pathway in addition to the existing sidewalk.
- **PDF A-46:** The paseo will introduce consistent landscaping and lighting that will provide a cohesive and improved visual appearance across the Project site.

Utility Equipment

- **PDF A-47:** All utility service equipment, including but not limited to meters, vaults, sprinkler risers, vacuum breakers, and all service and trash areas shall be screened from neighboring properties and public right-of-way and shall be located away from major pedestrian routes and outdoor seating areas.
- **PDF A-48:** All utility service equipment shall be screened by landscape materials including trees, shrubs, and groundcover and/or fences or walls designed to conform to the standards outlined within the proposed LAX Northside Design Guidelines and Standards.
- **PDF A-49:** No materials, supplies or equipment, including trucks or other motor vehicles (excluding company vehicles for passenger use) shall be stored on-site unless located inside a closed building or screened from public view.
- **PDF A-50:** Roof mounted equipment shall be screened at a minimum equal to the height of the equipment.
- **PDF A-51:** Walls designed to screen utilitarian equipment shall be a maximum of six (6) feet in height, measured from finish grade.
- **PDF A-52:** Areas dedicated to loading shall not be visible from a public street.

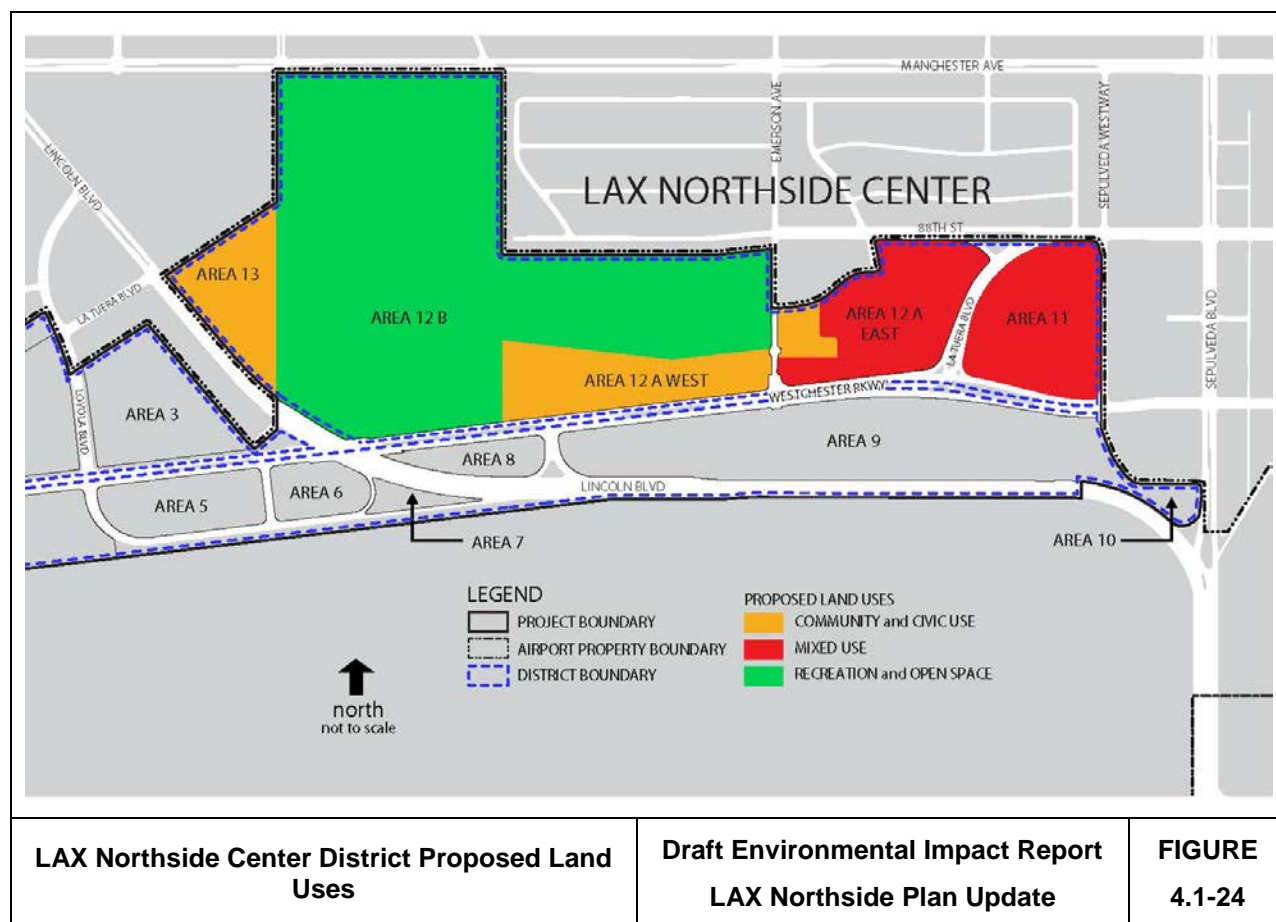
District/Area-Specific Project Design Features

Specific standards for land use, height, setbacks, stepbacks, buffers, and building locations for areas within each proposed Project District are described below.

LAX Northside Center District

The LAX Northside Center District is located adjacent to existing retail and commercial development. Proposed land uses allowed in this district are intended as an extension of those that currently exist in the Westchester Business District. Proposed land uses for the LAX Northside Center District reflect a mix of moderate intensity commercial development including retail, shopping, dining, hotel, and office (**Figure 4.1-24**). The LAX Northside Center District is envisioned as a pedestrian oriented commercial setting on the east end of the Project site that is intended to complement and enhance the Westchester Business District. Existing uses in this District (i.e., the Los Angeles Fire Department Station Number 5, the Westchester Golf Course, and the First Flight Childcare Center), as well as existing soundwalls, would remain. Signage in the Northside Center District is restricted to four and a half feet high. The Northside Center District will establish a maximum building square footage of 645,000 square feet.

4.1 Aesthetics



Area 11

Area 11 is designated for Commercial Mixed Use land uses. Specific Project Design Features related to aesthetics applicable to Area 11 include:

- **PDF A-53:** Building heights in Area 11 are restricted to no more than 60 feet (approximately five stories).
- **PDF A-54:** Buildings are prohibited within the Limited Development Area.
- **PDF A-55:** Buildings in Area 11 are required to be set back:
 - 15 feet from Sepulveda Westway;
 - 30 feet from South La Tijera Boulevard; and
 - 50 feet from the Limited Development Area.
- **PDF A-56:** Buildings within Area 11 are required to be located adjacent to the Westchester Parkway, La Tijera Boulevard, and Sepulveda Westway setbacks.
- **PDF A-57:** Buildings located adjacent to the 88th Street and La Tijera Boulevard property line are required to be stepped back by one foot for each additional foot of height above 15 feet.

Area 12A East

Area 12A East is designated for Commercial Mixed Use in the undeveloped portion and Community and Civic use in the portion currently developed with LAFD Fire Station Number 5. Specific Project Design Features related to aesthetics applicable to Area 12A East include:

- **PDF A-58:** Building heights in Area 12A East are restricted to no more than 60 feet (approximately five stories).
- **PDF A-59:** Buildings in Area 12 A East are required to be set back:
 - 15 feet from La Tijera/West 88th Place;
 - 18 feet from Westchester Parkway;
 - 20 feet from the south and west edges of existing structures; and
 - 30 feet from West 88th Street.
- **PDF A-60:** Buildings within Area 12A East are required to be located adjacent to the Westchester Parkway, La Tijera Boulevard, and Sepulveda Westway setbacks.

Area 12A West

Area 12A West is designated for Community and Civic use. Specific Project Design Features related to aesthetics applicable to Area 12A West include:

- **PDF A-61:** Building heights in Area 12A West are restricted to no more than 20 feet (approximately two stories).
- **PDF A-62:** Buildings in Area 12A West are required to be set back:
 - 15 feet from Westchester Parkway; and
 - 20 feet from the south and west edges of existing structures.
- **PDF A-63:** Buildings within Area 12A West are required to be located adjacent to the Westchester Parkway setback.

Area 12B

Area 12B is designated for Recreation and Open Space. The existing Westchester Golf Course and associated clubhouse would remain in Area 12B in their existing location and configuration. The proposed Project does not include any Project Design Features for height limits or setbacks for Area 12B.

Area 13

Area 13 is designated for Community and Civic use. The existing First Flight Child Development Center would remain in its existing location and configuration. Specific Project Design Features related to aesthetics applicable to Area 13 include:

- **PDF A-64:** Building heights in Area 13 are restricted to no more than 45 feet (approximately four stories).
- **PDF A-65:** Buildings in Area 13 are required to be set back:
 - 15 feet from Lincoln Boulevard; and
 - 20 feet from the south and west edges of the existing structures.

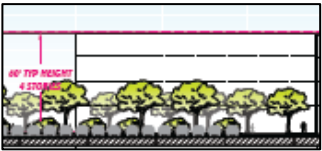
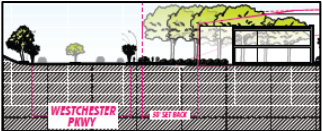
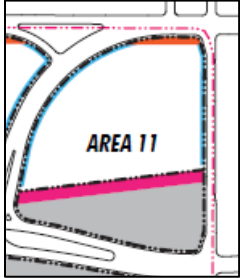
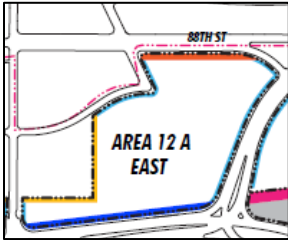
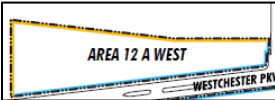
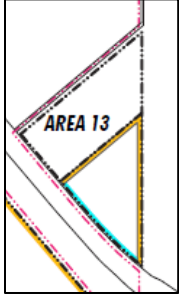
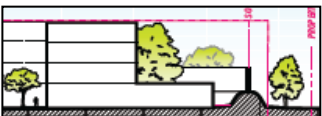
4.1 Aesthetics

- **PDF A-66:** Buildings within Area 13 are required to be located adjacent to the Westchester Parkway, La Tijera Boulevard, and Sepulveda Westway setbacks.

Table 4.1-1 summarizes the major Project Design Features related to massing and scale for the LAX Northside Center District.

Table 4.1-1

LAX Northside Center District Project Design Features

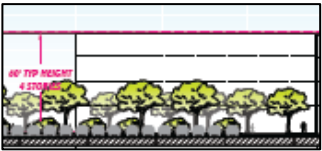
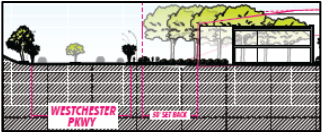



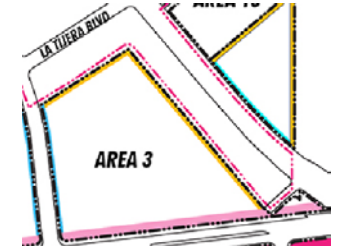
Project Design Feature Category	Area 11	Area 12A East	Area 12A West	Area 12B	Area 13
Building Height 	Maximum 60 Feet or 5 Stories	Maximum 60 Feet or 5 Stories	Maximum 20 Feet or 2 Stories	Not Applicable (No New Development Proposed)	Maximum 45 Feet or 4 Stories
Setback  REQUIRED SETBACKS 15' SETBACK 18' SETBACK 20' SETBACK 30' SETBACK 38' SETBACK 50' SETBACK 60' SETBACK RUNWAY SETBACK	 15' from Sepulveda Westway 30' from S La Tijera Blvd 50' from Limited Development Area	 15' from La Tijera Blvd & W 88 th Pl 18' from Westchester Pkwy 20' on the south & west edges of existing buildings 30' from W. 88 th Street	 15' from Westchester Pkwy 20' from the south and west edges of existing structures	Not Applicable (No New Development Proposed)	 15' from Lincoln Bl 20' from the south and west edges of existing structures
Stepback 	Buildings located adjacent to the 88 th St and La Tijera Blvd property line are required to be stepped back by 1 foot for each additional foot of height above 15 feet	No Requirements	No Requirements	Not Applicable (No New Development Proposed)	No Requirements

Source: RCHS, 2014.

4.1 Aesthetics

Table 4.1-2


LAX Northside Campus District Project Design Features

Project Design Feature Category	Area 1	Area 2	Area 3
Building Height 	Maximum 45 Feet or 4 Stories	Maximum 45 Feet or 4 Stories	Maximum 60 Feet or 5 Stories
Setback  REQUIRED SETBACKS 	 30' from Falmouth Avenue 38' from Westchester Parkway 80' from the Buffer	 Area 2A 15' St Bernard/W. 91 St./S Cum Laude Ave. 20' W. Cum Laude Ave. & eastern edges 30' Falmouth Ave. 38' Westchester Pkwy. Areas 2C-2D 20' North & west & east edges 38' Westchester Pkwy Areas 2E 15' Loyola Blvd. 20' North & west edges 38' Westchester Pkwy.	 15' from Loyola Blvd 20' from the north and west edges of the Area 38' from Westchester Pkwy
Landscaped Buffer	20-foot-wide Buffer along northern end	100 foot-wide Buffer along northern end of subareas 2C, 2D, and 2E	No Requirements

Source: RCHS, 2014.

Table 4.1-3

LAX Northside Airport Support District Project Design Features

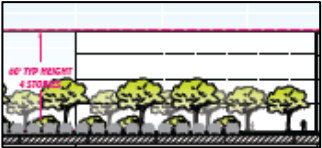
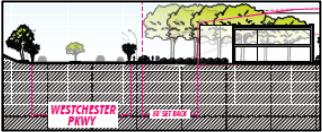
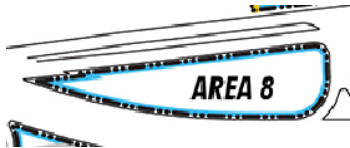
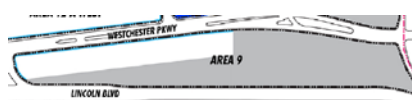

Project Design Feature Category	Area 4	Area 5	Area 6	Area 7
<p>Building Height</p> 	<p>Maximum 30 Feet</p> <p>or</p> <p>2 Stories</p>	<p>Maximum 30 Feet</p> <p>or</p> <p>2 Stories</p>	<p>Maximum 30 Feet</p> <p>or</p> <p>2 Stories</p>	<p>Maximum 30 Feet</p> <p>or</p> <p>2 Stories</p>
<p>Setback</p>  <p>REQUIRED SETBACKS</p> <ul style="list-style-type: none"> 15' SETBACK 18' SETBACK 20' SETBACK 30' SETBACK 38' SETBACK 50' SETBACK 80' SETBACK RUNWAY SETBACK 	 <p>15' from Northside Parkway</p> <p>20' from the southern edge of the Area</p> <p>50' from South Pershing Drive and Westchester Parkway</p>	 <p>15' from Northside Parkway/Georgetown Avenue/McClean Parkway</p> <p>50' from Westchester Parkway</p>	 <p>15 feet from Northside Parkway/Georgetown Avenue/McClean Parkway</p> <p>50' from Westchester Parkway</p>	 <p>15' from Lincoln Boulevard/McClean Parkway</p>

Source: RCHS, 2014.

4.1 Aesthetics

Table 4.1-3

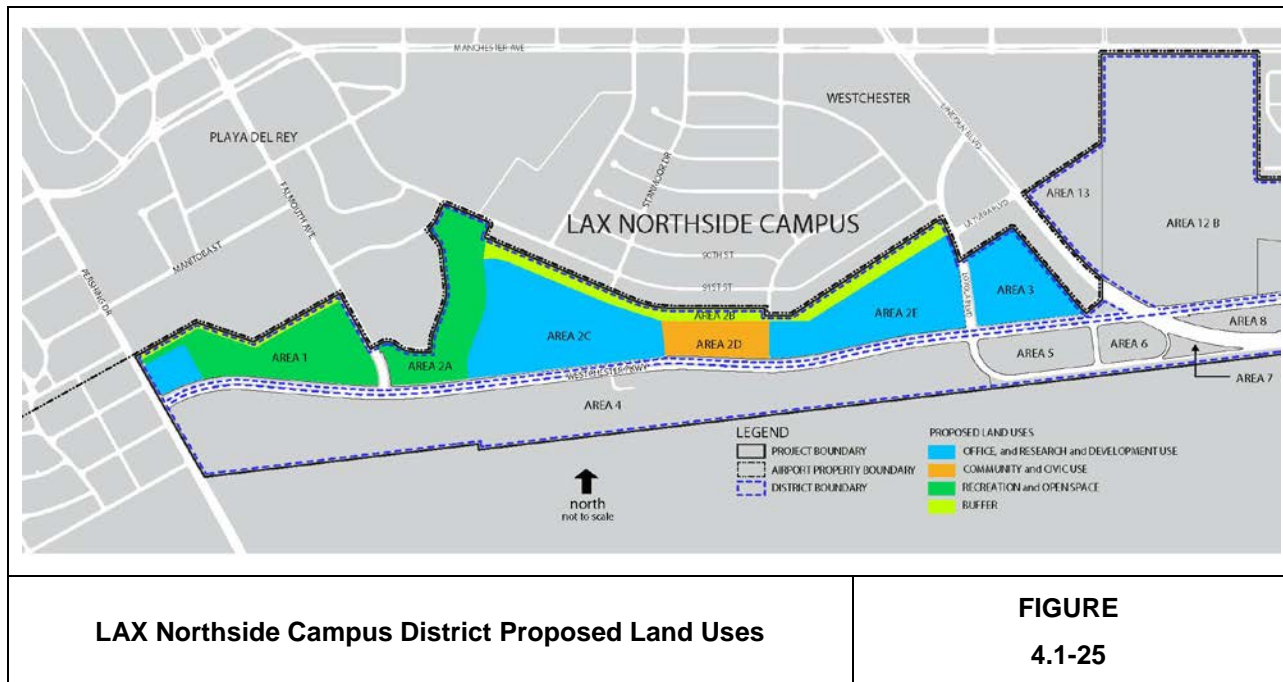
LAX Northside Airport Support District Project Design Features

Project Design Feature Category	Area 8	Area 9	Area 10
<p>Building Height</p> 	<p>Maximum 30 Feet</p> <p>or</p> <p>2 Stories</p>	<p>Maximum 30 Feet</p> <p>or</p> <p>2 Stories</p>	<p>Maximum 30 Feet</p> <p>or</p> <p>2 Stories</p>
<p>Setback</p>  <p>REQUIRED SETBACKS</p> <ul style="list-style-type: none"> 15' SETBACK 18' SETBACK 20' SETBACK 30' SETBACK 38' SETBACK 50' SETBACK 60' SETBACK RUNWAY SETBACK 	 <p>15' from all edges of Area 8.</p>	 <p>15' from Westchester Parkway and South McConnel Avenue.</p>	 <p>Entire Area is within the Limited Development Area.</p>

Source: RCHS, 2014.

LAX Northside Campus District

The LAX Northside Campus District is envisioned as a low intensity, low-rise, creative campus flanked by open space to the west and buffer space to the north (**Figure 4.1-25**). The campus is intended to attract research and development, higher education, technology, media, governmental offices and use, and/or other creative economy and office uses. New recreational space, which can only be developed in conjunction with other commercial uses at the Project site, is proposed for the westernmost portions of the Project site and could potentially include playing fields, a dog park, and open space. Along the north side of Westchester Parkway, buildings are intended to maintain a relationship to the street, but will be allowed flexibility in terms of siting, design and clustering. Minimal building setbacks are required outside of internal areas so as to reinforce the pedestrian scale internal to each development. Signage in the Northside Campus District is restricted to three feet in height. The Northside Center District will establish a maximum building square footage of 1,075,000. The Project site will be graded to provide a more accessible relationship with Westchester Parkway.



Area 1

Area 1 is designated for Office, Research and Development and Recreation and Open Space uses. The existing Jet Pets Animal Quarantine Facility would remain in its existing location and configuration. Specific Project Design Features related to aesthetics applicable to Area 1 include:

- **PDF A-67:** Building heights in Area 1 are restricted to no more than 45 feet.
- **PDF A-68:** A 20-foot-wide Buffer is designated along the northern edge of Area 1.
 - Buildings are prohibited within the Buffer.
 - The Buffer is required to be secured by a fence that matches landscaping.
 - Trees planted in the Buffer are required to be spaced to minimize obstruction of views from adjacent residences.

4.1 Aesthetics

- **PDF A-69:** Buildings in Area 1 are required to be set back:
 - 30 feet from Falmouth Avenue;
 - 38 feet from Westchester Parkway; and
 - 80 feet from the Buffer.
- **PDF A-70:** Existing grading will be preserved to separate this area from the busy nature of Westchester Parkway.

Area 2

Area 2 is designated for Office, Research and Development, Buffer, Community and Civic, and Recreation and Open Space uses. Specific Project Design Features related to aesthetics applicable to Area 2 include:

- **PDF A-71:** Building heights in Area 2 are restricted to no more than 45 feet.
- **PDF A-72:** In subarea 2A, buildings are required to be set back:
 - 15 feet from St. Bernard/West 91st Street/South Cum Laude Avenue;
 - 20 feet from West Cum Laude Avenue and the eastern edge of the subarea;
 - 30 feet from Falmouth Avenue; and
 - 38 feet from Westchester Parkway.
- **PDF A-73:** In subareas 2C and 2D buildings are required to be set back:
 - 20 feet from the north and west edges of the subareas; and
 - 38 feet from Westchester Parkway.
- **PDF A-74:** In subarea 2E buildings are required to be set back:
 - 15 feet from Loyola Boulevard;
 - 20 feet from the north and west edges of the subarea; and
 - 38 feet from Westchester Parkway.
- **PDF A-75:** Subarea 2B is designated as a Buffer use along that extends along the north of subareas 2C, 2D, and 2E and directly south of existing residential uses.
 - Buildings are prohibited in this 100-foot wide area.
 - The Buffer is required to be secured by a fence that matches landscaping.
- **PDF A-76:** Buildings within Area 2 are required to be located adjacent to the Westchester Parkway, La Tijera Boulevard, and Sepulveda Westway setback.
- **PDF A-77:** Buildings within subareas 2C and 2E are required to be located with a minimum of 65 percent of the proposed Project ground floor building square footage within 250 feet of the Westchester Parkway property line.
- **PDF A-78:** Grading in Area 2 will decrease elevations for building frontages relative to existing residential development and provide a more accessible relationship with Westchester Parkway.

Area 3

Area 3 is designated for Office, Research and Development uses. Specific Project Design Features related to aesthetics applicable to Area 3 include:

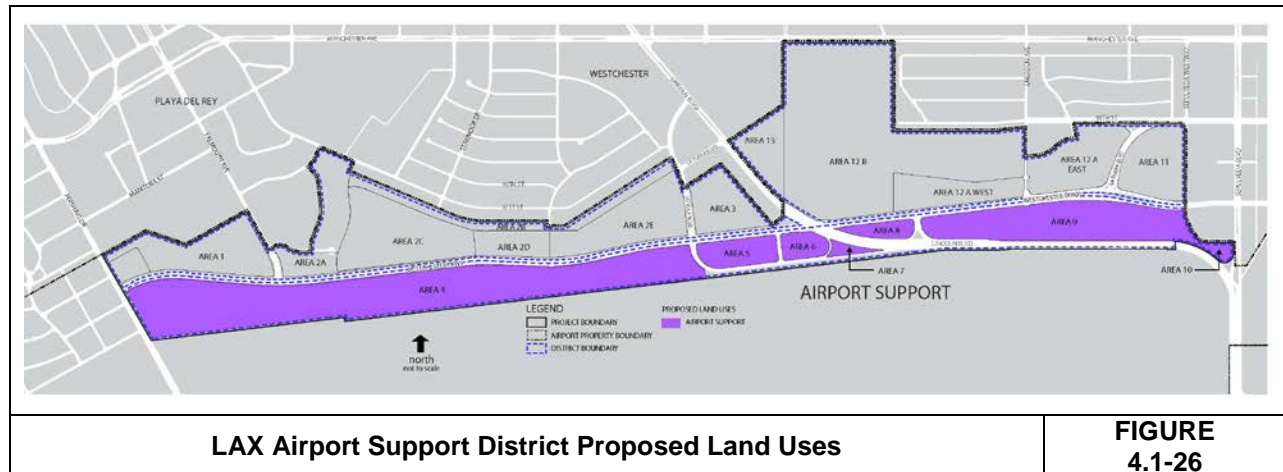
- **PDF A-79:** Building heights in Area 3 are restricted to no more than 60 feet.
- **PDF A-80:** Buildings are required to be set back:
 - 15 feet from Loyola Boulevard;
 - 20 feet from the north and west edges of the Area; and
 - 38 feet from Westchester Parkway.
- **PDF A-81:** Grading in Area 3 will decrease elevations for building frontages relative to existing residential development and provide a more accessible relationship with Westchester Parkway.

LAX Airport Support District

The LAX Airport Support District areas are all located south of Westchester Parkway. Given their proximity to the LAX North Airfield and the existing airport radar equipment, private commercial development is not proposed for this area. The LAX Airport Support District would allow airport support uses including, but not limited to maintenance and repair shops, indoor storage and warehouses, exterior storage and warehouses, parking, and administrative office (**Figure 4.1-26**). The LAX Airport Support District will establish a maximum building square footage of 600,000. Specific Project Design Features related to aesthetics generally applicable to all areas of the LAX Airport Support District include:

- **PDF A-82:** All Airport Support District Areas have a building height restriction of no more than 30 feet.
- **PDF A-83:** Buildings are prohibited within the Limited Development Area.
- **PDF A-84:** Aircraft engine testing is prohibited.
- **PDF A-85:** Grading strategies and landscape berms will be preserved as they exist today and will work to limit the visual presence of the LAX Airport Support District from the view of neighbors north of Westchester Parkway.
- **PDF A-86:** Where applicable, additional grading may be introduced in the LAX Airport Support District to further enhance landscape berms.

4.1 Aesthetics



Area 4

Specific Project Design Features related to aesthetics applicable to Area 4 include:

- **PDF A-87:** Buildings in Area 4 are required to be set back:
 - 15 feet from Northside Parkway;
 - 20 feet from the southern edge of the Area; and
 - 50 feet from South Pershing Drive and Westchester Parkway.

Area 5

Specific Project Design Features related to aesthetics applicable to Area 5 include:

- **PDF A-88:** Buildings in Area 5 are required to be set back:
 - 15 feet from Northside Parkway/Georgetown Avenue/McClean Parkway; and
 - 50 feet from Westchester Parkway.

Area 6

Specific Project Design Features related to aesthetics applicable to Area 6 include:

- **PDF A-89:** Buildings in Area 6 are required to be set back:
 - 15 feet from Northside Parkway/Georgetown Avenue/McClean Parkway; and
 - 50 feet from Westchester Parkway.

Area 7

Specific Project Design Features related to aesthetics applicable to Area 7 include:

- **PDF A-90:** Buildings in Area 7 are required to be set back 15 feet from Lincoln Boulevard/McClean Parkway.

Area 8

Specific Project Design Features related to aesthetics applicable to Area 8 include:

- **PDF A-91:** Buildings in Area 8 are required to be set back 15 feet from all edges of Area 8.

Area 9

Specific Project Design Features related to aesthetics applicable to Area 9 include:

- **PDF A-92:** Buildings in Area 9 are required to be set back 15 feet from Westchester Parkway and South McConnel Avenue.

Area 10

Specific Project Design Features related to aesthetics applicable to Area 10 include:

- **PDF A-93:** Buildings in Area 10 are prohibited within Area 10 as the entire Area is within the Limited Development Area.

4.1.3.4 Project Impacts

4.1.3.4.1 Visual Character Impacts

Aesthetics

Removal/Alteration of Valued Visual Character

Construction

LAX Northside Center District

Area 11

Area 11 is currently used as a temporary construction staging area for LAX projects (**Figure 4.1-27**). Current construction operations lend the Project site a disorderly form as equipment is irregularly spaced throughout. The undeveloped nature of Area 11 is characterized by mostly exposed ground with ruderal/weedy vegetation and some trees. One major access road bisects the eastern portion of Area 11. The topography is primarily flat, although there are some mounds of dirt. Additionally, there is a cluster of rectangular shaped mobile trailers and storage units within central portion of the Project site. The entire Area 11 is bounded by a chain-link fence with fabric and/or landscape that hides most of the equipment and activities at street level.

The proposed Project would allow Area 11 to transition from a largely undeveloped/vacant space to a fully developed site of urban character. Although Area 11 contains several trees, none of them qualify as mature trees (based on an 8-inch dbh) that may be considered a valued visual resource (Refer to 4.3 Biological Resources and Appendix F). Consequently, Area 11 does not have any known valued visual resources.

4.1 Aesthetics

Figure 4.1-27 – Existing Visual Character of Area 11, Aerial View



Source: Google Earth, 2014

Construction of the proposed Project in Area 11 would add construction equipment and increase construction activity. Construction period impacts associated with the proposed Project would act as an extension and intensification of the type of use that currently occurs within Area 11. Additional mobile trailers, grading equipment, cranes, tractors and other vehicles used in construction would act as an intensification of the site's current character. However, as Area 11 does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

Area 12A East

Area 12A East is primarily vacant except for the existing City of Los Angeles Fire Department (LAFD) Fire Station Number 5 on the southwestern portion of this Area (**Figure 4.1-28**). A chain link fence borders the vacant portions of Area 12A East that abut La Tijera Boulevard and Westchester Parkway, while vacant portions of Area 12A East that abut W 88th Street and West 88th Place have landscaped soundwalls. Area 12A East was previously developed as part of a residential subdivision and the vacant portion of this Area retains abandoned paved areas which served as roads as well as lighting fixtures from its former residential use. Aside from abandoned paved streets and lighting fixtures, the vacant portion of Area 12A East is sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with 10 mature trees. The 10 mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, the vacant portion of Area 12A East does not contain known or designated valued visual resources.

Area 12A East also includes LAFD Station Number 5, which was built in 2005 and is composed of two contemporary industrial-style buildings: a main building and a garage. The main building is two stories and the garage is one-story, and both have irregular-shaped plans. Both buildings have flat roofs covered with unknown materials and the main building features signal lights on the rooftop. The walls are clad with a combination of stucco, brick, and concrete block. Wall

features include scoring in the stucco, decorative metal panels, and metal awnings. The main building has aluminum-framed windows with sills and hoods, arranged symmetrically. Given that there is no architectural distinction of the building and that it has no historic association (being built in 2005), the existing LAFD Station Number 5 is not considered a valuable visual resource.

Construction of the proposed Project in Area 12A East would add construction equipment and construction activity to the vacant parts of it, but will not affect the existing LAFD Station Number 5. Mobile trailers, grading equipment, cranes, tractors and other vehicles used in construction would be introduced, which are not part of the existing visual character of Area 12A East. However, as Area 12A East does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

In addition, construction activities will include removal of all mature trees identified in Area 12A East. However, these trees are not designated heritage trees or protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6) and, therefore, are not known to be valued visual resources. Nevertheless, in accordance with LAX Master Plan MM-BC-3, any of the identified mature trees that will be impacted due to construction activities will require a 2:1 replacement. A replacement plan will be prepared and implemented by LAWA or its designee which requires replacement trees to be local native species of at least 15-gallon size, and that the replacement of trees shall be within the boundaries of LAX or at a suitable off-site location.

Figure 4.1-28 – Existing Visual Character of Area 12A East, Aerial View



Source: Google Earth, 2014

Area 12A West

Area 12A West is primarily vacant (**Figure 4.1-29**). A portion of Area 12A West has been and is being used as a temporary construction staging area. A chain link fence borders the portions of Area 12A West that abut Westchester Parkway, while portions of Area 12A West that abut the existing Westchester Golf Course are separated by vegetation and a dirt path. Area 12A West was previously developed as part of a residential subdivision and a portion of this Area retains abandoned paved areas which served as roads as well as lighting fixtures from its former residential use. Aside from abandoned paved streets and lighting fixtures, the vacant portion of

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Area 12A West is sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, Area 12A West does not contain known or designated valued visual resources.

Construction of the proposed Project in Area 12A West would add construction equipment and construction activity. Mobile trailers, grading equipment, cranes, tractors and other vehicles used in construction would be introduced, which are not part of the existing visual character of the majority of Area 12A West. However, as Area 12A West does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

In addition, construction activities will include removal of all mature trees identified in Area 12A West. However, these trees are not designated heritage trees or protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6) and, therefore, are not known to be valued visual resources. Nevertheless, in accordance with LAX Master Plan MM-BC-3, any of the identified mature trees that will be impacted due to construction activities will require a 2:1 replacement. A replacement plan will be prepared and implemented by LAWA or its designee which requires replacement trees to be local native species of at least 15-gallon size, and that the replacement of trees shall be within the boundaries of LAX or at a suitable off-site location.

Figure 4.1-29 – Existing Visual Character of Area 12A West, Aerial View



Source: Google Earth, 2014

Area 12B

Area 12B contains the existing Westchester Golf Course and associated clubhouse. The Westchester Golf Course clubhouse is a Spanish Colonial Revival-style building with a north-facing orientation. It is one-story with a rectangular plan and two distinct ends: a western half that serves as the restaurant and pro shop, and an eastern half used to store and wash golf carts. Although the Westchester Golf Course and/or the clubhouse may be considered valuable

visual resources, there is no proposed alteration or removal of these facilities. Furthermore, no construction staging would be permitted in Area 12B. Therefore, no construction impacts related to removal or alteration of valued visual resources would occur.

Area 13

Area 13 is split nearly evenly between vacant land and the existing First Flight Child Development Center (**Figure 4.1-30**). A chain link fence borders the vacant portions of Area 13 that abut Lincoln Boulevard as well as the existing Westchester Golf Course. There exists an 8-foot-tall soundwall along the vacant portions of Area 13 that abut the existing First Flight Child Development Center. A portion of Area 13 contains a parking lot with marked parking spaces and with security lighting. Aside from parking lot and lighting fixtures, the vacant portion of Area 13 is sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) but no mature trees (Refer to 4.3 Biological Resources and Appendix F). Therefore, the vacant portion of Area 13 does not contain known or designated valued visual resources.

Figure 4.1-30 – Existing Visual Character of Area 13, Aerial View



Source: Google Earth, 2014

Area 13 also includes the First Flight Child Development Center, which was built in 1999. The main building in the First Flight Child Development Center that is visible from the public vantage point is a one-story contemporary commercial building with an irregular plan. The contemporary commercial building features multiple angled rooflines and the roof is covered in corrugated metal sheet. Given that there is no architectural distinction of the building and that it has no historic association (being built in 1999), the existing First Flight Child Development Center is not considered a valuable visual resource.

Construction of the proposed Project in Area 13 would add construction equipment and construction activity to the vacant parts of it, but will not affect the existing First Flight Child Development Center. Mobile trailers, grading equipment, cranes, tractors and other vehicles used in construction would be introduced, which are not part of the existing visual character of Area 13. However, as Area 13 does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

4.1 Aesthetics

LAX Northside Campus District

Area 1

Area 1 is primarily vacant except for the existing Jet Pets Animal Quarantine Facility located on the southwestern portion of this Area (Figure 4.1-31). A chain link fence borders the vacant portions of Area 1 that abut Westchester Parkway, South Pershing Drive, and Falmouth Avenue. Vacant portions of Area 1 that abut the existing residential developments on the north boundary have landscape and fencing separating the two uses. The vacant portion of Area 1 is sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) but no mature trees (Refer to 4.3 Biological Resources and Appendix F). Therefore, the vacant portion of Area 1 does not contain known or designated valued visual resources.

Area 1 also includes the existing Jet Pets Animal Quarantine Facility, which began its current operations in 1973 in existing structures that were built starting in 1947. The main office has a rectangular plan and a south-facing orientation. The walls are constructed of painted concrete block. The main office building has a very low-pitch front-gabled and shed roof of composition shingle which features mounted lights and wooden signage. The boarding facility has a T-shaped plan and an east-facing orientation. The boarding facility walls are constructed of painted concrete block and the flat roof is covered with composition shingle. The associated office building has a rectangular plan and an east-facing orientation. The associated office building walls are constructed of painted concrete block and the flat roof is covered with composition shingle. Given that there is some association of the Jet Pets Animal Quarantine Facility with historical uses, it is potentially considered a valuable visual resource.

Figure 4.1-31 – Existing Visual Character of Area 1, Aerial View



Source: Google Earth, 2014

Construction of the proposed Project in Area 1 would add construction equipment and construction activity to the vacant parts of it, but will not affect the existing Jet Pets Animal Quarantine Facility. Mobile trailers, grading equipment, cranes, tractors and other vehicles used in construction would be introduced, which are not part of the existing visual character of Area 1. However, none of these construction activities would occur within the footprint of the existing Jet Pets Animal Quarantine Facility. Therefore, no construction impacts related to removal or alteration of valued visual resources would occur.

Area 2

Area 2 is primarily vacant (**Figure 4.1-32**). A chain link fence borders all of Area 2. Area 2 was previously developed as part of a residential subdivision and a portion of this Area retains abandoned paved areas which served as roads as well as lighting fixtures from its former residential use. Aside from abandoned paved streets and lighting fixtures, Area 2 is also sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, Area 2 does not contain known or designated valued visual resources.

Figure 4.1-32 – Existing Visual Character of Area 2, Aerial View



Source: Google Earth, 2014

Construction of the proposed Project in Area 2 would add construction equipment and construction activity. Mobile trailers, grading equipment, cranes, tractors and other vehicles used in construction would be introduced, which are not part of the existing visual character of the majority of Area 2. However, as Area 2 does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

In addition, construction activities will include removal of all mature trees identified in Area 2. However, these trees are not designated heritage trees or protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6) and, therefore, are not known to be valued visual resources. Nevertheless, in accordance with LAX Master Plan MM-BC-3, any of the identified mature trees that will be impacted due to construction activities will require a 2:1 replacement. A replacement plan will be prepared and implemented by LAWA or its designee which requires replacement trees to be local native species of at least 15-gallon size, and that the replacement of trees shall be within the boundaries of LAX or at a suitable off-site location.

Area 3

Area 3 is primarily vacant (**Figure 4.1-33**). A chain link fence borders all of Area 3. Area 3 was previously developed as part of a residential subdivision and a portion of this Area retains abandoned paved areas which served as roads as well as lighting fixtures from its former residential use. Aside from abandoned paved streets and lighting fixtures, Area 3 is also sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA

4.1 Aesthetics

standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, Area 3 does not contain known or designated valued visual resources.

Construction of the proposed Project in Area 3 would add construction equipment and construction activity. Mobile trailers, grading equipment, cranes, tractors and other vehicles used in construction would be introduced, which are not part of the existing visual character of the majority of Area 3. However, as Area 3 does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

Figure 4.1-33 – Existing Visual Character of Area 3, Aerial View



Source: Google Earth, 2014

In addition, construction activities will include removal of all mature trees identified in Area 3. However, these trees are not designated heritage trees or protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6) and, therefore, are not known to be valued visual resources. Nevertheless, in accordance with LAX Master Plan MM-BC-3, any of the identified mature trees that will be impacted due to construction activities will require a 2:1 replacement. A replacement plan will be prepared and implemented by LAWA or its designee which requires replacement trees to be local native species of at least 15-gallon size, and that the replacement of trees shall be within the boundaries of LAX or at a suitable off-site location.

LAX Northside Airport Support District

Area 4

Area 4 is primarily vacant and with disturbed soils, and is also used as a temporary construction staging area for LAX projects (**Figure 4.1-34**). Current construction operations lend the Project site a disorderly form as equipment is irregularly spaced throughout. The undeveloped nature of Area 4 is characterized by mostly exposed ground with ruderal/weedy vegetation. The topography is primarily flat, although there are some mounds of dirt. Additionally, there is a

cluster of rectangular shaped mobile trailers and storage units within western portion of the Area 4. The entire Area 4 is bounded by a chain-link fence. The proposed Project would allow Area 4 to transition from a largely undeveloped/ vacant space a fully developed site of urban character. Area 4 does not have any known valued visual resources.

Construction of the proposed Project in Area 4 would add construction equipment and increase construction activity. Construction period impacts associated with the proposed Project would act as an extension and intensification of the type of use that currently occurs within Area 4. Additional mobile trailers, grading equipment, cranes, tractors and other vehicles used in construction would act as an intensification of the site's current character. However, as Area 4 does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

Figure 4.1-34 – Existing Visual Character of Area 4, Aerial View



Source: Google Earth, 2014

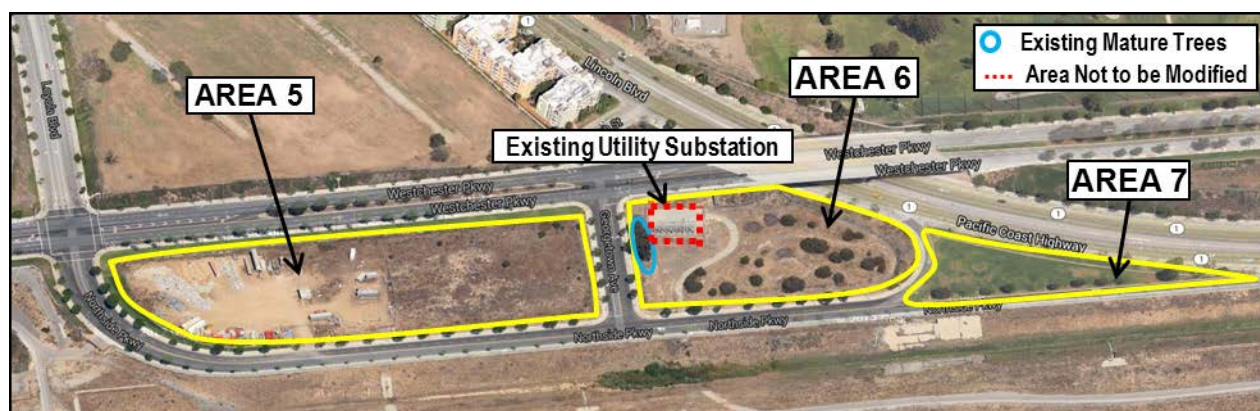
Area 5

Area 5 is primarily vacant and with disturbed soils, and is also used as a temporary construction staging area for LAX projects (**Figure 4.1-35**). Current construction operations lend the Project site a disorderly form as equipment is irregularly spaced throughout. The undeveloped nature of Area 5 is characterized by mostly exposed ground with ruderal/weedy vegetation. The topography is primarily flat, although there are some mounds of dirt. Additionally, there is a cluster of rectangular shaped mobile trailers and storage units within western portion of the Area 5. The entire Area 5 is bounded by a chain-link fence. The proposed Project would allow Area 5 to transition from a largely undeveloped/ vacant space to a fully developed site of urban character. Area 5 does not have any known valued visual resources.

Construction of the proposed Project in Area 5 would add construction equipment and increase construction activity. Construction period impacts associated with the proposed Project would act as an extension and intensification of the type of use that currently occurs within Area 5. Additional mobile trailers, grading equipment, tractors, and other vehicles used in construction would act as an intensification of the site's current character. However, as Area 5 does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

4.1 Aesthetics

Figure 4.1-35 – Existing Visual Character of Areas 5, 6, and 7, Aerial View



Source: Google Earth, 2014

Area 6

Area 6 is primarily vacant and with disturbed soils, but also contains an existing utilities substation (**Figure 4.1-35**). A chain link fence borders Area 6, which is also sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, the vacant portions of Area 6 do not contain known or designated valued visual resources.

Area 6 also includes an existing electric utilities substation. The substation is small and the electrical equipment rests on a concrete pad. There is fencing surrounding the substation but no fabric or landscaping obstructs views into the substation. The topography is a sloping one from north to southeast. Electrical utilities substations are not considered valued visual resources. This use will not be modified as part of the proposed Project.

Construction of the proposed Project in Area 6 would add construction equipment and activity to the vacant parts of it, but will not affect the existing electric utilities substation. Mobile trailers, grading equipment, tractors, and other vehicles used in construction would be introduced, which are not part of the existing visual character of Area 6. However, as Area 6 does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

In addition, construction activities will include removal of all mature trees identified in Area 6. However, these trees are not designated heritage trees or protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6) and, therefore, are not known to be valued visual resources. Nevertheless, in accordance with LAX Master Plan MM-BC-3, any of the identified mature trees that will be impacted due to construction activities will require a 2:1 replacement. A replacement plan will be prepared and implemented by LAWA or its designee which requires replacement trees to be local native species of at least 15-gallon size, and that the replacement of trees shall be within the boundaries of LAX or at a suitable off-site location.

Area 7

Area 7 is primarily vacant and landscaped (**Figure 4.1-35**). The topography is sloping, from north to southeast. Area 7 does not have restricted access and primarily serves as a

landscaped area at the junction of two roads, Lincoln Boulevard and Northside Parkway. The proposed Project would allow Area 7 to transition from a vacant space to a fully developed site of urban character. Area 7 does not have any known valued visual resources.

Construction of the proposed Project in Area 7 would add construction equipment and activities to this Area. Mobile trailers, grading equipment, tractors, and other vehicles used in construction would be introduced, which are not part of the existing visual character of Area 7. However, as Area 7 does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

Area 8

Area 8 is primarily vacant (**Figure 4.1-36**). A chain link fence borders all of Area 8. Area 8 is sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, Area 8 does not contain known or designated valued visual resources.

Construction of the proposed Project in Area 8 would add construction equipment and activities. Mobile trailers, grading equipment, tractors, and other vehicles used in construction would be introduced, which are not part of the existing visual character of the majority of Area 8. However, as Area 8 does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

Figure 4.1-36 – Existing Visual Character of Areas 8, 9, and 10, Aerial View



Source: Google Earth, 2014

In addition, construction activities will include removal of all mature trees identified in Area 8. However, these trees are not designated heritage trees or protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6) and, therefore, are not known to be valued visual resources. Nevertheless, in accordance with LAX Master Plan MM-BC-3, any of the identified mature trees that will be impacted due to construction activities will require a 2:1 replacement. A replacement plan will be prepared and implemented by LAWA or its designee which requires replacement trees to be local native species of at least 15-gallon size, and that the replacement of trees shall be within the boundaries of LAX or at a suitable off-site location.

Area 9

Area 9 includes vacant land and an existing FAA radar facility (**Figure 4.1-36**). A chain link fence borders all of Area 9. Area 9 was previously developed as part of a residential subdivision

4.1 Aesthetics

and a portion of this Area retains abandoned paved areas which served as roads as well as lighting fixtures from its former residential use. Aside from abandoned paved streets and lighting fixtures, Area 9 is also sparsely vegetated with native grasses (which are regularly maintained by LAWA, per FAA standards) and with several mature trees. The mature trees include palms, and deciduous and evergreen trees (Refer to 4.3 Biological Resources and Appendix F). None of the trees are designated as heritage trees or are protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6). Therefore, Area 9 does not contain known or designated valued visual resources.

Area 9 also includes an existing FAA radar facility utilized at LAX, which was constructed between 1978 and 1982. It is composed of three small and one large one-story buildings, as well as a metal rotating structure. The large building is L-shaped and the other three buildings have a rectangular footprint. All four buildings feature flat roofs covered with unknown material. The walls are constructed of concrete block with some wall vents. All of the buildings lack windows, and their entries contain single and double metal doors with small concrete stoops. The radar is a metal rotating structure atop a metal scaffold structure. The radar facility is enclosed by a perimeter chain link fence and features a paved surface parking lot. This use will not be modified as part of the proposed Project.

Construction of the proposed Project in Area 9 would add construction equipment and activities to the vacant parts of it, but will not affect the existing radar facility. Mobile trailers, grading equipment, tractors, and other vehicles used in construction would be introduced, which are not part of the existing visual character of Area 9. However, as Area 9 does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

In addition, construction activities will include removal of all mature trees identified in Area 9. However, these trees are not designated heritage trees or protected under the City of Los Angeles Preservation of Protected Trees Ordinance (Los Angeles Municipal Code Chapter IV, Article 6) and, therefore, are not known to be valued visual resources. Nevertheless, in accordance with LAX Master Plan MM-BC-3, any of the identified mature trees that will be impacted due to construction activities will require a 2:1 replacement. A replacement plan will be prepared and implemented by LAWA or its designee which requires replacement trees to be local native species of at least 15-gallon size, and that the replacement of trees shall be within the boundaries of LAX or at a suitable off-site location.

Area 10

Area 10 is primarily vacant and landscaped (**Figure 4.1-36**). The topography is generally flat. Area 10 does not have restricted access and primarily serves as a landscaped area at the junction of two roads, Lincoln Boulevard and Sepulveda Boulevard. The proposed Project would allow Area 10 to transition from a vacant space to a fully developed site of urban character. Area 10 does not have any known valued visual resources.

Construction of the proposed Project in Area 10 would add construction equipment and activities to this Area. Mobile trailers, grading equipment, tractors, and other vehicles used in construction would be introduced, which are not part of the existing visual character of Area 10. However, as Area 10 does not contain any known valued visual resources, no construction impacts related to removal or alteration of valued visual resources would occur.

Operation

LAX Northside Center District

Area 11

Under the proposed Project, the type of development that would be allowed in Area 11 is Commercial Mixed-Use. Operations of the proposed Project would include typical operations associated with commercial and office uses, such as parking, product delivery, and irrigation of landscape. As discussed under the Construction Impacts analysis, Area 11 does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 12A East

Under the proposed Project, the type of development that would be allowed in Area 12A East is Commercial Mixed-Use in the undeveloped portions. Operations of the proposed Project would include typical operations associated with commercial and office uses, such as parking, product delivery, and irrigation of landscape. As discussed under the Construction Impacts analysis, Area 12A East does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 12A West

Under the proposed Project, the type of development that would be allowed in Area 12A West is Community and Civic. Operations of the proposed Project would include typical operations associated with community and civic uses, such as parking, product delivery, and irrigation of landscape. As discussed under the Construction Impacts analysis, Area 12A West does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 12B

Under the proposed Project, Area 12B, which is the existing Westchester Golf Course, would not be modified. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 13

Under the proposed Project, the type of development that would be allowed in Area 13 is Community and Civic in the undeveloped portion. Operations of the proposed Project would include typical operations associated with community and civic uses, such as parking, product delivery, and irrigation of landscape. As discussed under the Construction Impacts analysis, Area 13 does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

LAX Northside Campus District

Area 1

Under the proposed Project, the type of development that would be allowed in Area 1 is Office, Research and Development and Recreation and Open Space in the vacant areas. Operations of the proposed Project would include typical operations associated with Office, Research and Development and Recreation and Open Space uses, such as parking, product delivery, irrigation of landscape, and recreational activities. As discussed under the Construction Impacts analysis, the vacant portions of Area 1 do not have any known valued visual resources.

4.1 Aesthetics

Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 2

Under the proposed Project, the type of development that would be allowed in Area 2 is Office, Research and Development, Buffer, Community and Civic, and Recreation and Open Space. Operations of the proposed Project would include typical operations associated with Office, Research and Development and Recreation and Open Space uses, such as parking, product delivery, irrigation of landscape, and recreational activities. As discussed under the Construction Impacts analysis, Area 2 does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 3

Under the proposed Project, the type of development that would be allowed in Area 3 is Office, Research and Development. Operations of the proposed Project would include typical operations associated with Office, Research and Development uses, such as parking, product delivery, and irrigation of landscape. As discussed under the Construction Impacts analysis, Area 3 does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

LAX Northside Airport Support District

Area4

Under the proposed Project, the type of development that would be allowed in Area 4 is Airport Support. Operations of the proposed Project would include typical operations associated with Airport Support uses, such as parking, product delivery, and mechanical and maintenance work. As discussed under the Construction Impacts analysis, Area 4 does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 5

Under the proposed Project, the type of development that would be allowed in Area 5 is Airport Support. Operations of the proposed Project would include typical operations associated with Airport Support uses, such as parking, product delivery, and mechanical and maintenance work. As discussed under the Construction Impacts analysis, Area 5 does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 6

Under the proposed Project, the type of development that would be allowed in Area 6 is Airport Support. Operations of the proposed Project would include typical operations associated with Airport Support uses, such as parking, product delivery, and mechanical and maintenance work. As discussed under the Construction Impacts analysis, Area 6 does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 7

Under the proposed Project, the type of development that would be allowed in Area 7 is Airport Support. Operations of the proposed Project would include typical operations associated with Airport Support uses, such as parking, product delivery, and mechanical and maintenance work. As discussed under the Construction Impacts analysis, Area 7 does not have any known valued

visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 8

Under the proposed Project, the type of development that would be allowed in Area 8 is Airport Support. Operations of the proposed Project would include typical operations associated with Airport Support uses, such as parking, product delivery, and mechanical and maintenance work. As discussed under the Construction Impacts analysis, Area 8 does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 9

Under the proposed Project, the type of development that would be allowed in Area 9 is Airport Support. Operations of the proposed Project would include typical operations associated with Airport Support uses, such as parking, product delivery, and mechanical and maintenance work. As discussed under the Construction Impacts analysis, Area 9 does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Area 10

Under the proposed Project, the type of development that would be allowed in Area 10 is Airport Support. Operations of the proposed Project would include typical operations associated with Airport Support uses, such as parking, product delivery, and mechanical and maintenance work. As discussed under the Construction Impacts analysis, Area 10 does not have any known valued visual resources. Therefore, no operational impacts related to removal or alteration of valued visual resources would occur.

Visual Character Integration/Contrast

Construction

Although temporary in nature, construction activities generally cause a contrast to, and disruption in the general order and aesthetic character of an area. Project construction activities include grading, clearing, and grubbing the land; installing utilities, building foundations, superstructures, and building skin/roofing; completing interior framing and finishing; installing hardscape and landscaping; and building testing/commissioning. Construction equipment includes, but is not limited to, drill rigs, cement and mortar mixers, forklifts, graders, cranes, and tractors. All construction activities would comply with LAX Master Plan Commitment DA-1, which requires construction fencing to screen construction areas. Temporary construction fencing would be placed along the periphery of the development sites of the proposed Project to screen much of the construction activity from view at the street level. Impacts related to visual character integration and contrast by District, by Area are discussed below.

LAX Northside Center District

Area 11

During construction of the proposed Project, the visual appearance of Area 11 would be consistent with the existing visual character. Area 11 includes outdoor storage of construction materials as well as temporary bungalows. Construction fencing already exists along the east, west, and south of Area 11. The existing soundwall along the northern edge of Area 11 and La Tijera Boulevard would remain during construction and would screen construction activities from residences to the north. Construction of buildings on Area 11 is required to be set back 15 feet

4.1 Aesthetics

from Sepulveda Avenue/La Tijera. Additionally, building construction within Area 11 is required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback. Construction activities would be visible to pedestrians and motorists on adjacent streets, as well as to viewers within nearby commercial buildings to the east. However, construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, would comply with the Project Design Features related to setbacks and building location, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 11 would be less than significant.

Area 12A East

During construction of the proposed Project, the visual appearance of Area 12A East would be altered due to the removal of abandoned roads and street lights. Area 12A East is vacant except for LAFD Station Number 5. LAFD Station Number 5 would remain in its current location and configuration. The existing soundwall along the northern edge of Area 12A East would also remain in its current location and configuration and would screen construction activities from residences to the north. Construction of buildings on Area 12A East is required to be set back 30 feet from West 88th Street and 15 feet from La Tijera/West 88th Place. Additionally, building construction within Area 12A East is required to be located adjacent to the Westchester Parkway and La Tijera setback. Construction activities would be visible to pedestrians and motorists on adjacent streets, however construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, would comply with the Project Design Features related to setbacks and building location, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 12A East would be less than significant.

Area 12A West

During construction of the proposed Project, the visual appearance of Area 12A West would be altered due to the removal of abandoned roads and existing vegetation. Buildings within Area 12A West are required to be located adjacent to the Westchester Parkway setback. Construction activities would be visible to pedestrians and motorists on adjacent streets, however construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, would comply with Project Design Features related to building location, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 12A West would be less than significant.

Area 12B

No construction activities would occur on Area 12B. The existing Westchester Golf Course would remain in its existing location and configuration. Therefore, no construction impacts to visual character integration/contrast on Area 12B would occur.

LAX Northside Campus District

Area 1

During construction of the proposed Project, the visual appearance of Area 1 would be altered due to the removal of the existing vegetation. The existing Jet Pets Animal Quarantine Facility would remain in its current location and configuration and no construction activity would occur

on that portion of Area 1. A 20 foot wide Buffer is designated along the northern edge of Area 1 that would separate construction activities from adjacent residences. Additionally, building construction is required to be set back 80 feet from the Buffer, for a total 100 feet of separation of building construction from existing residences. Construction activities would be visible to pedestrians and motorists on Pershing Drive, Westchester Parkway, and Falmouth Avenue and from the balconies of multi-family residences to the north. However, views onto Area 1 from adjacent roads are limited due to the roads being generally lower in elevation than Area 1, views from residential uses are partially screened due to existing mature trees, construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, would comply with Project Design Features related to Buffers and setbacks, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 1 would be less than significant.

Area 2

During construction of the proposed Project, the visual appearance of Area 2 would be altered due to the removal of the existing vegetation and trees, removal of existing roads and street lights from previous development, and grading. Construction activities would be visible to pedestrians and motorists on Westchester Parkway, Falmouth Avenue, and Loyola Boulevard and from residences to the north. However, views onto Area 2 from adjacent roads are limited due to the roads being generally lower in elevation than Area 2, views from residential uses are partially screened due to private block walls. Construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 2 would be less than significant.

Area 3

During construction of the proposed Project, the visual appearance of Area 3 would be altered due to the removal of the existing unused parking area and associated light poles. The First Flight Child Development Center would remain in its existing location and configuration and no construction activities would occur on that portion of Area 3. An existing block wall, construction fencing, and landscaping, screen Area 3 from the Westchester Golf Course and First Flight Child Development Center. The proposed Project requires a 100-foot Buffer along the northern edge of Area 2 adjacent to residences and a 20 foot setback from the Buffer where building construction is prohibited. Additionally, building construction within Area 2 is required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback. Construction activities would be visible to pedestrians and motorists on Lincoln Boulevard, however construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, would comply with Project Design Features related to Buffers and setbacks, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 2 would be less than significant.

4.1 Aesthetics

LAX Northside Airport Support District

Area 4

During construction of the proposed Project, the visual appearance of Area 4 would be consistent with the existing visual character. Area 4 includes outdoor storage of construction materials as well as temporary bungalows. Visibility of Area 4 is limited due to the fact that it is higher in grade than Pershing Drive and some parts of Westchester Parkway. Existing landscaping along Westchester Parkway would remain. Construction activities would be visible to pedestrians and motorists on Westchester Parkway, however construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, would comply with the Project Design Features related to setbacks and building location, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 4 would be less than significant.

Area 5

During construction of the proposed Project, the visual appearance of Area 5 would be consistent with the existing visual character. Area 5 includes outdoor storage of construction materials. Construction fencing already exists along the perimeter of Area 5. Construction activities would be visible to pedestrians and motorists on adjacent streets, however construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, would comply with the Project Design Features related to setbacks and building location, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 5 would be less than significant.

Area 6

During construction of the proposed Project, the visual appearance of Area 6 would be altered as landscaping could be removed and construction materials may be introduced. Construction activities would be visible to pedestrians and motorists on Westchester Parkway, Northside Parkway, and Georgetown Avenue, however construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, would comply with the Project Design Features related to setbacks and building location, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 6 would be less than significant.

Area 7

During construction of the proposed Project, the visual appearance of Area 7 would be altered as landscaping could be removed and construction materials may be introduced. Construction activities would be visible to pedestrians and motorists on Lincoln Boulevard and Northside Parkway, however construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, would comply with the Project Design Features related to setbacks and building location, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 7 would be less than significant.

Area 8

During construction of the proposed Project, the visual appearance of Area 8 would be altered as landscaping could be removed and construction materials may be introduced. Existing landscaping along Westchester Parkway would remain and Area 8 is not visible from Lincoln Boulevard as it is higher in elevation than Lincoln Boulevard. Construction activities would be visible to pedestrians and motorists on Westchester Parkway, however construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, would comply with the Project Design Features related to setbacks and building location, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 8 would be less than significant.

Area 9

During construction of the proposed Project, the visual appearance of Area 9 would be altered as landscaping could be removed and construction materials may be introduced. Existing landscaping along Westchester Parkway would remain. Construction activities would be visible to pedestrians and motorists on Westchester Parkway, Lincoln Boulevard, and Sepulveda Westway, however construction activities would not substantially alter or degrade the existing visual character of the site, would not generate substantial long-term contrast with the visual character of the surrounding area, would be screened at the street level by construction fencing, would comply with the Project Design Features related to setbacks and building location, and would be temporary in nature. Therefore, construction impacts to visual character integration/contrast on Area 9 would be less than significant.

Area 10

Area 10 is located entirely within the Limited Development Area Zone. No construction activities would occur on Area 10. Therefore, no construction impacts to visual character integration/contrast on Area 10 would occur.

Operation

Visual character is drawn from both natural and urban features. Natural features include native landscape or ornamental vegetation, heritage oak trees, and distinct topographic or geologic features. Urban features include structures of architectural or historic significance and structures with visual prominence. Differences in these elements generate visual contrast. The following analysis assesses the degree to which the proposed Project may significantly or adversely affect the existing visual character of each Area and its valued aesthetic image.

LAX Northside Center District

Area 11

Operation of the proposed Project would alter the existing visual character of the site by allowing the site to transition from a largely undeveloped/vacant space to that of an area possessing urban qualities. Currently, construction operations lend the site a disorderly form as equipment is irregularly spaced throughout the site. Dominant colors include warm shades of brown during the dry months of the year. The dominant color is drawn from the undeveloped nature of Area 11, which is mostly exposed ground with ruderal/weedy vegetation. One major access road currently bisects the eastern portion of Area 11. The topography is mostly flat. Area 11 is separated from residences to the north by an existing sound wall (**Figure 4.1-37**).

4.1 Aesthetics

Area 11 is designated for Commercial Mixed Use land uses. Building heights in Area 11 are restricted to no more than 60 feet. Buildings are prohibited within the Limited Development Area. Buildings are required to be set back 50 feet from the Limited Development Area, 30 feet from South La Tijera Avenue, and 15 feet from Sepulveda Avenue/La Tijera. Additionally, buildings within Area 11 are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback. Buildings located adjacent to the 88th Street and La Tijera property line are required to be stepped back by one foot for each additional foot of height above 15 feet.

In addition to the setback standards noted above, the proposed Project would adopt standards to control building massing, height, façade design, landscaping, and requirements for a public paseo that will connect Sepulveda Boulevard with Pershing Drive. This paseo includes provisions for landscaping and lighting that will further augment the current form, line, color and texture of the site. The paseo must be a minimum of 12 feet between the existing sidewalk within the 50-foot building setback in Area 11 along the runway protection zone. The provision of these setbacks and minimum width requirements of the paseo will add a strong east/west trending line through Area 11. New structures will be capped at a height of 60 feet, adding vertical three dimensional mass to what is mostly vacant. Such structural development will be restricted to development envelopes that establish setbacks to minimize the potential for adverse changes to existing character of views.

While the proposed Project will alter the character of this vacant parcel, the proposed Project will have a net benefit on the existing character Area 11. Operation of Area 11 will cohesively blend with the character of existing commercial type development that is located just to the east of Area 11. Therefore, the operational impacts to visual character integration/contrast at Area 11 will be less than significant.

Figure 4.1-37 – Existing View From Intersection of La Tijera and West 88th Street



Note: View faces east. Soundwall and landscaping flanking at Area 11 are visible in right area of image.
Source: URS Corporation, 2014

Area 12A East

Area 12 A East is currently vacant except for the existing Fire Station Number 5. A chain link fence roughly borders portions of the parcel that abut La Tijera and Westchester Parkway, while a landscaped soundwall abuts portions of the Area that abut W 88th Street and West 88th Place. The site is bisected by several abandoned streets. Area 12A East is bordered by a residential subdivision to the north, vacant parcels to the east, south and west. Vacant parcels are sparsely

vegetated, and were previously developed. An existing soundwall on the north side of Area 12A East creates a visual separation between the Area and the residential neighborhood to the north.

Area 12A East is designated for Commercial Mixed Use in the undeveloped portion, and Community and Civic use in the portion currently developed as the LAFD Fire Station Number 5. Building heights in Area 12A East are restricted to no more than 60 feet. Buildings are required to be set back 30 feet from West 88th Street, 18 feet from Westchester Parkway, 15 feet from La Tijera/West 88th Place, and 20 feet on the north and west edges of existing buildings. Additionally, buildings within Area 12A East are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback.

The proposed Project would allow development of Area 12A East that will add visual mass and dimension to the site. Development will be limited to areas within a defined development envelope that is intended to reduce potential visual contrast with surrounding uses. Provisions for landscape buffers, façade treatments and materials, and roofing will further ensure the proposed Project does not contrast with surrounding development. Furthermore, the existing soundwall will remain in its current location and configuration and would screen the majority of operational activities from residential uses north of Area 12A East.

While the proposed Project will alter the character of Area 12A East as it appears from neighboring vacant parcels, the current character of Area 12A East will remain intact as viewed from residential areas to the north. Furthermore, the proposed Project will have a net benefit on the existing character of Area 12A East by adding structural dimension, mass, and landscaping in an organized and planned manner. Operation of Area 12A East will cohesively blend with the character of existing commercial uses to the east. Therefore, operational impacts to visual character integration/contrast at Area 12A East will be less than significant.

Area 12A West

Area 12A West is currently vacant. It is located immediately south of a portion of Area 12B, and a soundwall is located on the north side of Area 12B, directly south of W 88th Street. Area 12A West was previously developed. A residential subdivision is located north of the soundwall in Area 12B. The Westchester Golf Course borders the west side of Area 12A West, while vacant parcels are located to the south.

Area 12A West is designated for Community and Civic use. Building heights in Area 12A West are restricted to no more than 20 feet. Buildings are required to be set back 30 feet from West 88th Street, 18 feet from Westchester Parkway, 15 feet from La Tijera/West 88th Place, and 20 feet on the north and west edges of existing buildings. Additionally, buildings within Area 12A West are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback.

The proposed Project would allow development of Area 12A West that will add visual mass and dimension to the site. Development will be limited to areas within a defined development envelope that is intended to reduce potential visual contrast with surrounding uses. Provisions for landscape buffers, façade treatments and materials, and roofing will further ensure the proposed Project does not negatively contrast with surrounding development. Furthermore, the existing soundwall will remain in its current location and configuration and would screen the majority of operational activities from residential uses north of Area 12A West.

While the proposed Project will alter the character of Area 12A West as it appears from neighboring vacant parcels and the Westchester Golf Course, the current character of Area 12A West will remain intact as viewed from residential areas to the north. Furthermore, the proposed

4.1 Aesthetics

Project will have a net benefit on the existing character of Area 12A West by adding structural dimension, mass, and landscaping in an organized and planned manner. Operation of Area 12A West will cohesively blend with the existing character of the area. Therefore, the operational impacts to visual character integration/contrast at Area 12A West will be less than significant.

Area 12B

Area 12B is designated for Recreation and Open Space. The existing Westchester Golf Course and associated clubhouse would remain in this area in their existing location and configuration. Therefore, no operational impacts to visual character integration/contrast at Area 12B would occur.

Area 13

Area 13 contains an unused parking lot with associated light posts and vegetation. Area 13 is designated for Community and Civic use. The existing First Flight Child Development Center would remain in its existing location and configuration. Building heights in Area 13 are restricted to no more than 45 feet. Buildings are required to be set back 15 feet from Lincoln Boulevard and 20 feet from the north and east edges of the Area. Additionally, buildings within Area 13 are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback.

While the proposed Project will alter the visual character of the undeveloped portion of Area 13 as it appears from Lincoln Boulevard and the Westchester Golf Course by adding structural dimension, mass, and landscaping, the proposed Project development would be integrated with the visual character of the existing First Flight Child Development Center. Operation of Area 13 will cohesively blend with the existing character of the area. Therefore, the operational impacts to visual character integration/contrast at Area 13 will be less than significant.

LAX Northside Campus District

Area 1

Area 1 is mostly vacant aside from the existing Jet Pets facility and access road. Multifamily residential uses abut the northern edge of Area 1, vacant parcels are located to the east, south, and west sides. The topography of Area 1 is variable and peaks toward the western half of the site.

Area 1 is designated for Office, Research and Development and Recreation and Open Space uses. The existing Jet Pets animal quarantine facility would remain in its existing location and configuration. Building heights in Area 1 are restricted to no more than 45 feet. A 20 foot wide Buffer is designated along the northern edge of Area 1. Buildings are prohibited within the Buffer. The Buffer is required to be secured by a fence that matches landscaping. Trees planted in the Buffer are required to be spaced to minimize obstruction of views from adjacent residences. Buildings are required to be set back 80 feet from the Buffer, 30 feet from Falmouth Avenue, and 38 feet from Westchester Parkway. Existing grading will be preserved to separate this area from the busy nature of Westchester Parkway.

The proposed Project would allow development of Area 1 that will add minor increases in visual mass and dimension to the site. Development will be limited to areas within a defined development envelope that is intended to reduce potential visual contrast with surrounding uses. Provisions for landscape buffers, fencing of recreational uses, and lighting standards will further ensure the proposed Project does not negatively contrast with surrounding residential development.

While the proposed Project will alter the character of Area 1 as it appears from neighboring residential uses and vacant parcels, the development will occur in a manner that does not contrast with the neighboring residential development and will be consistent with the existing Jet Pets structures. Operation of Area 1 will cohesively blend with the existing character of the area. Therefore, the operational impacts to visual character integration/contrast at Area 1 will be less than significant.

Area 2

Operation of the proposed Project would alter the existing visual character of the site by allowing the site to transition from a largely undeveloped/vacant space to that of an area possessing urban qualities. Currently, the site is mostly undeveloped and contains abandoned streets, light poles, and trees. A residential community is located north of Area 2, while vacant parcels border the east, south and west. The residential community to the north is visually separated from Area 2 by a fence.

Area 2 is designated for Office, Research and Development, Buffer, Community and Civic, and Recreation and Open Space uses. Building heights in Area 2 are restricted to no more than 45 feet. In subarea 2A, buildings are required to be set back 15 feet from St. Bernard/West 91st Street/South Cum Laude Avenue, 20 feet from West Cum Laude Avenue and the eastern edge of the subarea, 30 feet from Falmouth Avenue, and 38 feet from Westchester Parkway. In subareas 2C and 2D, buildings are required to be set back 20 feet from the north and west edges of the subareas and 38 feet from Westchester Parkway. In subarea 2E, buildings are required to be set back 15 feet from Loyola Boulevard, 20 feet from the north and west edges of the subarea, and 38 feet from Westchester Parkway. Subarea 2B is designated as a Buffer use that extends along the north of subareas 2C, 2D, and 2E and directly south of existing residential uses. Buildings are prohibited in this 100-foot wide area. The Buffer is required to be secured by a fence that matches landscaping. Additionally, buildings within Area 2 are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback. Buildings within subareas 2C and 2E are required to be located with a minimum of 65 percent of the proposed Project ground floor building square footage within 250 feet of the Westchester Parkway property line. Grading in this area will decrease elevations for building frontages relative to existing residential development and provide a more accessible relationship with Westchester Parkway.

In addition to the setback standards noted above, the proposed Project would adopt standards to control building massing, height, façade design, landscaping, and require the development of active recreation/ public open space at the peak elevation of this Area. The proposed Project would allow development of Area 2 that will add visual mass and dimension to the site. Development will be limited to areas within a defined development envelope that is intended to reduce potential visual contrast with surrounding uses. Provisions for landscape buffers, façade treatments and materials, and roofing will further ensure the proposed Project does not negatively contrast with surrounding residential development. Furthermore, the existing wall separating Area 2 from the residential community to the north will remain in its current location and configuration, and would screen the majority of operational activities from residential uses north of Area 2.

While the proposed Project will alter the character of Area 2, the proposed Project will have a net benefit on the existing character of Area 2 and its surrounding environs. Operation of Area 2 will cohesively blend with the character of existing character of the area. Therefore, the operational impacts to visual character integration/contrast at Area 2 will be less than significant.

4.1 Aesthetics

Area 3

Area 3 is bordered by multifamily residential development to the north and east, and vacant parcels to the south and west. The multifamily residential development is roughly 40 feet tall. The multifamily unit structures are angular and represent a prominent three dimensional form. Vacant parcels are characterized by rolling terrain with sparse vegetation, the texture of which is coarse.

Area 3 is designated for Office, Research and Development uses. Building heights in Area 3 are restricted to no more than 60 feet. Buildings are required to be set back 15 feet from Loyola Boulevard, 20 feet from the north and west edges of the Area, and 38 feet from Westchester Parkway. Grading in this area will decrease elevations for building frontages relative to existing residential development and provide a more accessible relationship with Westchester Parkway.

The proposed Project would allow development of Area 3 that will add visual mass and dimension to the site. Development will be limited to areas within a defined development envelope that is intended to reduce potential visual contrast with surrounding uses. Provisions for landscape buffers, façade treatments and materials, and roofing will further ensure the proposed Project does not negatively contrast with surrounding development.

While the proposed Project will alter the character of Area 3 as it appears from neighboring vacant parcels and the multifamily residences, the proposed Project will have a net benefit on the existing character of Area 3 by adding structural dimension, mass, and landscaping in an organized and planned manner. Operation of Area 3 will cohesively blend with the existing character of the area. Therefore, the operational impacts to visual character integration/contrast at Area 3 will be less than significant.

LAX Northside Airport Support District

Area 4

Area 4 is currently characterized by one-story airport support structures and outdoor storage of construction materials. Area 4 is designated for Airport Support uses. Building heights are restricted to no more than 30 feet. The proposed Project would allow development of structures similar in height and massing to existing structures, and consistent with the visual character of the LAX North Airfield and related LAX structures to the south. Operation of Area 4 will cohesively blend with the existing character of the area. Therefore, the operational impacts to visual character integration/contrast at Area 4 will be less than significant.

Area 5

Area 5 is currently characterized by outdoor storage of construction materials and construction fencing that surrounds the perimeter of the Area. Area 5 is designated for Airport Support uses. Building heights are restricted to no more than 30 feet. The proposed Project would allow development of structures similar in height and massing to existing structures located to the west on Area 4, and consistent with the visual character of the LAX North Airfield and related LAX structures to the south. Continued use of Area 5 for construction laydown would also be consistent with the existing visual character, which includes the presence of construction materials. Operation of Area 5 will cohesively blend with the existing character of the area. Therefore, the operational impacts to visual character integration/contrast at Area 5 will be less than significant.

Area 6

Area 6 is currently characterized by vegetation and utility equipment. Area 5 is designated for Airport Support uses. Building heights are restricted to no more than 30 feet. The proposed

Project would allow development of structures similar in height and massing to existing structures located to the west on Area 4, and consistent with the visual character of the LAX North Airfield and related LAX structures to the south. Use of Area 5 for construction laydown would also be consistent with the existing visual character on adjacent Area 5, which includes the presence of construction materials. Operation of Area 6 will cohesively blend with the existing character of the area. Therefore, the operational impacts to visual character integration/contrast at Area 6 will be less than significant.

Area 7

Area 7 is currently characterized by existing landscaping. Area 7 is designated for Airport Support uses. Building heights are restricted to no more than 30 feet. The proposed Project would allow development of structures similar in height and massing to existing structures located to the west on Area 4, and consistent with the visual character of the LAX North Airfield and related LAX structures to the south. Use of Area 7 for construction laydown would also be consistent with the existing visual character on adjacent Airport properties, which includes the presence of construction materials. Operation of Area 7 will cohesively blend with the existing character of the area. Therefore, the operational impacts to visual character integration/contrast at Area 7 will be less than significant.

Area 8

Area 8 is currently characterized by existing landscaping. Area 8 is designated for Airport Support uses. Building heights are restricted to no more than 30 feet. The proposed Project would allow development of structures similar in height and massing to existing structures located to the west on Area 4, and consistent with the visual character of the LAX North Airfield and related LAX structures to the south. Use of Area 8 for construction laydown would also be consistent with the existing visual character on adjacent Airport properties, which includes the presence of construction materials. Existing perimeter landscaping along Westchester Parkway and Lincoln Boulevard would remain on Area 8, further integrating the visual character of the proposed Project with surrounding areas. Operation of Area 8 will cohesively blend with the existing character of the area. Therefore, the operational impacts to visual character integration/contrast at Area 8 will be less than significant.

Area 9

Area 9 is currently characterized by existing vegetation, paved areas, and the Airport radar facility. Area 9 is designated for Airport Support uses. Building heights are restricted to no more than 30 feet. The proposed Project would allow development of structures similar in height and massing to existing structures located to the north on Area 12A East, and consistent with the visual character of the LAX North Airfield and related LAX structures to the south. Use of Area 9 for construction laydown would also be consistent with the existing visual character on adjacent Airport properties, which includes the presence of construction materials. The existing Airport radar facility and existing perimeter landscaping along Westchester Parkway and Lincoln Boulevard would remain on Area 9, further integrating the visual character of the proposed Project with surrounding areas. Operation of Area 9 will cohesively blend with the existing character of the area. Therefore, the operational impacts to visual character integration/contrast at Area 9 will be less than significant.

Area 10

Area 10 is currently characterized by landscaping, including trees, ornamental vegetation, and grass. Area 10 is located completely within the Limited Development Area and operation of the

4.1 Aesthetics

proposed Project would not introduce any new structures or uses on Area 10. Therefore, no operational impacts to visual character integration/contrast at Area 10 would occur.

Attainment of Aesthetics Regulations

Construction

Construction of the proposed Project would comply with all applicable regulations. Therefore, no construction impacts related to attainment of aesthetic regulations would occur.

Operation

As analyzed in **Table 4.1-4**, the proposed Project would be consistent with the goals, policies, and objectives of the City of Los Angeles General Plan, the City of Los Angeles Zoning and Municipal Code, and the LAX Plan. Therefore, no operational impacts related to attainment of aesthetic regulations would occur.

Table 4.1-4

Attainment of Aesthetic Regulations

Goal, Policy or Objective	Analysis of Project Consistency		
	LAX Northside Center District	LAX Northside Campus District	LAX Northside Airport Support District
City of Los Angeles General Plan Framework			
Land Use Element: Provide for the siting and design of new development that maintains the prevailing scale and character of the City's stable residential neighborhoods and enhance the character of commercial and industrial districts.	Consistent: The LAX Northside Center District sites new development adjacent to Westchester Parkway and La Tijera Boulevard and set back from existing residences. New Mixed Use Commercial uses are designed to enhance the character of the Westchester Business District. Building heights are compatible with existing commercial uses along Sepulveda Boulevard.	Consistent: The LAX Northside Campus District sites new Office, Research and Development uses adjacent to Westchester Parkway and set back and buffered from residences to the north.	Consistent: The LAX Northside Airport Support District sites new development adjacent to existing airport uses, thus enhancing the character of adjacent areas.
Land Use Element Policy 5.7.1: Establish standards for transition in building height and for on-site landscape buffers.	Consistent: The LAX Northside Center District establishes height limits of 60' for Area 11 and 12A East, 20' for Area 12A West, and 45' for Area 13 (area 12B would remain as is).	Consistent: The LAX Northside Campus District establishes height limits of 45' for Areas 1 and 2 and 60' for Area 3. A 100 foot landscape Buffer separates portions of Area 2 from residences located directly adjacent to the north.	Consistent: The LAX Northside Airport Support District establishes building height limits of 30'.

4.1 Aesthetics

Table 4.1-4

Attainment of Aesthetic Regulations

Goal, Policy or Objective	Analysis of Project Consistency		
	LAX Northside Center District	LAX Northside Campus District	LAX Northside Airport Support District
City of Los Angeles Zoning and Municipal Code			
Section 93.0117: Regulates light spillover in residential areas. No exterior light source may cause more than two foot candles of lighting intensity or generate direct glare onto exterior glazed windows or glass doors; elevated habitable porch, deck, or balcony; or any ground surface intended for uses such as recreation, barbecue or lawn areas or any other property containing a residential unit or units.	Consistent: The LAX Northside Center District lighting will comply with LAMC Section 93.0117. Additionally, Project Design Features include designing lighting to prohibit light spillover.	Consistent: The LAX Northside Campus District lighting will comply with LAMC Section 93.0117. Additionally, Project Design Features include designing lighting to prohibit light spillover.	Not Applicable: The LAX Northside Airport Support District is not adjacent to residential areas.
LAX Plan			
Policy P1: Provide and maintain landscaped buffer areas along the northern boundary of Airport and northern boundary of LAX Northside that include setbacks, landscaping, screening, or other appropriate mechanisms with the goal of avoiding land use conflicts, shielding lighting, enhancing privacy, and better screening views or airport facilities from adjacent residential areas.	Consistent: Development proposed for the LAX Northside Center District would maintain the existing soundwalls that provide privacy for neighbors to the north of Area 11 and Area 12A East. Additionally, the building setback within Area 11 would ensure that building massing would step back as it increases in height to maintain privacy.	Consistent: The LAX Northside Campus District is planned for a 100-foot landscaped buffer located along the northern property line of Parcel 2, and a 20-foot landscaped buffer is proposed along the northwest property line in Parcel 1. This buffer provides a visual screen that physically separates proposed land uses within the LAX Northside Campus District from adjacent residential uses.	Not Applicable: The Airport Support District is located along the southern boundary of the LAX Northside, south of Westchester Parkway and is not adjacent to residential uses. Grading strategies and existing landscape berms will be preserved and will serve to limit the visual presence of this area from the residential community located north of Westchester Parkway and the Project site.

Table 4.1-4

Attainment of Aesthetic Regulations

Goal, Policy or Objective	Analysis of Project Consistency		
	LAX Northside Center District	LAX Northside Campus District	LAX Northside Airport Support District
Goal 5: Acknowledge neighborhood context and promote compatibility between LAX and the surrounding neighborhoods.	Consistent: Development proposed for the LAX Northside Center District will be oriented towards Westchester Parkway so that building massing is located around the edges of Areas 11, 12A East, 12A West, and Area 13 and away from the residential neighborhoods to the north.	Consistent: The entire Project site is intended to provide a buffer between LAX and surrounding neighborhoods. The LAX Northside Campus District is planned for a 100-foot landscaped buffer located along the northern property line of Parcel 2, and a 20-foot landscaped buffer is proposed along the northwest property line in Parcel 1. This buffer provides a visual screen that physically separates proposed land uses within the LAX Northside Campus District from adjacent residential uses.	Consistent: The Airport Support District is solely proposed for airport support uses and is located immediately adjacent to LAX. This district is separated from the rest of the proposed Project by Westchester Parkway, maintaining a clear distance from neighborhoods located north of the Project site.

Source: URS, 2014.

4.1.3.4.2 View Impacts**Obstruction of Recognized Views**

According to the City of Los Angeles CEQA Guidelines, the term “views” generally refers to visual access to, or the visibility of, a particular sight from a given vantage point or corridor.

The Project site is located in the vicinity of locally valued scenic resources. The following describes the potential “Aesthetic” related impacts that could occur to those resources as a result of the adoption of the proposed LAX Northside Design Guidelines and Standards and the development scenarios this document would allow.

Dockweiler Beach State Park

Based on the height limitations included in the proposed LAX Northside Design Guidelines and Standards, future development at the Project site would not be visible from Dockweiler Beach

4.1 Aesthetics

State Park. As discussed above, the east side of Dockweiler Beach State Park is flanked by Vista del Mar Highway, and the Los Angeles Airport/EI Segundo Dunes which are just east of the highway. Due to the comparative low elevation of Dockweiler Beach State Park in relation to the Los Angeles Airport/EI Segundo Dunes, views toward the east from Dockweiler Beach State Park are largely obstructed by the hillsides of the Los Angeles Airport/EI Segundo Dunes. In addition, views of Dockweiler Beach State Park from residences to the north of the Project site are located at such a distance and elevation, that the majority of these residents are not able to see Dockweiler State Beach from their residences. Therefore, the proposed Project would not have impacts to views of (representing recreational viewers) or from (representing residential viewers) Dockweiler Beach State Park.

Vista del Mar

Similar to the discussion of views from Dockweiler State Beach, views from Vista del Mar Highway toward the Project site and views from residences to the north of the Project site of Vista del Mar are already obstructed by the Los Angeles Airport/EI Segundo Dunes. Therefore, the proposed Project would not have impacts related to views of Vista del Mar.

Westchester Bluffs

Views of the Westchester Bluffs would remain unaffected as the Project site is not located between residences and this view resource. Therefore, the proposed Project would not have impacts related to views of or from the Westchester Bluffs.

Summary

While the proposed Project is located in the vicinity of the valued scenic resources discussed above, the proposed Project would not occur within or adjacent to a valued focal or panoramic vista, or within the view of any designated scenic highway, corridor or parkway. Furthermore, the proposed Project would not obstruct, interrupt, or diminish a valued focal and/or panoramic view as defined in the Community Plan.

Obstruction of Views

Construction

LAX Northside Center District

Area 11

Area 11 is bordered by La Tijera Boulevard to the north and west, Sepulveda Westway to the east, and Westchester Parkway to the south. Area 11 contains no documented views of valued scenic resources such as structures or natural landscapes. A single-family residential neighborhood abuts the north side of La Tijera.

Residences are assumed to have high levels of potential sensitivity to changes in visual character. Existing views toward Area 11 from these residences are largely obstructed by a sequence of two soundwalls which are separated by La Tijera. The first soundwall abuts the northern right-of-way of West 88th Street and La Tijera Drive. This soundwall separates the residential community from the public right of way. This soundwall acts as an initial obstruction, blocking the majority of structures beyond it from the vision of viewers in these residences. This is based on the principal that the elevation of this wall imposes an obstacle to view all but the tallest structures behind the wall.

Another soundwall is located on the north side of Area 11 which further obstructs any potential direct views of Area 11 from these residents. Therefore, construction activities within Area 11 are expected to be imperceptible to these residents because construction activities will not alter the existing design or location of these soundwalls. Therefore, the nature and character of these residents' current views will not change, nor will proposed Project construction interrupt or obstruct their existing views.

No other sensitive viewer groups are located in close proximity to Area 11, and therefore views from the east, south, and west directions of Area 11 are not discussed in this analysis.

Area 12A East

Area 12A East is bordered by 88th Street to the north, La Tijera to the west, Westchester Parkway to the south, and Emerson Ave to the west. Area 12A East contains no documented views of valued scenic resources such as structures or natural landscapes.

The same single family residential neighborhood described in Area 11 abuts the northern boundary of W 88th Street which is the only viewer group assessed in this analysis. Existing views toward Area 12A East from these residences are largely obstructed by a sequence of two soundwalls which are separated by West 88th Street. The first soundwall abuts the northern right-of-way of West 88th Street and the southern property line of the residences just north of this roadway. This soundwall acts as a visual obstruction blocking the majority of views of any structures behind it. The fact that these residents' views are mostly blocked is based on the principal that the elevation of this wall imposes an obstacle, beyond which the residents are located too close to see above or beyond. Therefore, construction activities within Area 12A East are expected to be imperceptible to residents. Construction activities will not alter the existing design or location of these soundwalls, therefore the nature and character of these residents' current views will not change, nor will proposed Project construction interrupt or obstruct their existing views.

No other sensitive viewer groups are located in close proximity to Area 12A East, and therefore views from the east, south, and west directions of Area 12A East are not discussed in this analysis.

Area 12A West

Area 12A West is bordered by Area 12B to the north, Emerson Ave to the east, Westchester Parkway to the south, and the Westchester Golf Course to the west. Area 12A West contains no documented views of valued scenic resources such as structures or natural landscapes. A single family neighborhood is located north of Area 12B and is separated from Area 12B by a soundwall and West 88th Street.

Residences are assumed to have high levels of potential sensitivity to changes in visual character. Existing views toward Area 12A West from these residences are largely obstructed by the soundwall located on the south side of West 88th Street, and by the vacant area within Area 12B (**Figure 4.1-38**). This soundwall would act as an initial obstruction, blocking the majority of structures beyond it from the vision of viewers in these residences. This is based on the principal that the elevation of this wall, combined with the vegetative screening in front of it, imposes an obstacle to view all but the tallest structures behind these elements. Therefore, construction activities within Area 12A West are expected to be imperceptible to these residents because construction activities will not alter the existing design of these soundwalls and vegetation. Therefore, the nature and character of these residents' current views will not change as a result of proposed Project construction activities, nor will proposed Project construction activities interrupt or obstruct these residents existing views.

4.1 Aesthetics

No other sensitive viewer groups are located in close proximity to Area 12A West, and therefore views from the east, south, and west directions of Area 12A West are not discussed in this analysis.

Figure 4.1-38 – Existing View From Front Yard of Residence on West 88th Street Toward Soundwall



Source: URS Corporation, 2014

Area 12B

Area 12B is designated for Recreation and Open Space (**Figure 4.1-39**). The existing Westchester Golf Course and associated clubhouse would remain in this area in their existing location and configuration. The proposed Project does not include any Project Design Features for height limits or setbacks for this Area. Therefore, no construction activities associated with the proposed Project will obstruct or interfere with existing views of Area 12B.

Figure 4.1-39 – Existing View of Area 12B, Westchester Golf Course



Source: URS Corporation, 2014

Area 13

Area 13 is bordered by Lincoln Boulevard to the west, the Westchester Golf Course to the east, and the Westchester Recreation Center to the north (**Figure 4.1-40**). Area 13 contains no documented views of valued scenic resources such as structures or natural landscapes. The existing First Flight Child Development Center would remain in its existing location. Were future development to occur in Area 13, construction equipment could obstruct or interfere with views from the Golf Course and Recreation Center toward urban uses to the west and LAX to the south. However, neither of these views are identified as scenic or valued. Therefore, proposed Project construction activities would not impact or obstruct views.

Figure 4.1-40 – Existing View of Area 13



Source: URS Corporation, 2014

LAX Northside Campus District

Area 1

Area 1 is bordered by apartment complexes to the north, Falmouth Avenue to the east, Westchester Parkway to the south and Pershing Drive to the west. Area 1 contains no documented views of valued scenic resources such as structures or landscapes, however the upper stories of the apartment complexes to the north and northeast of Area 1 may have views of the Pacific Ocean, which is a valued visual resource.

Existing grading in Area 1 will remain the same, and is reserved for recreational type uses. As such, construction of recreational uses such as soccer, lacrosse, baseball fields, or a dog park would introduce construction related equipment at the site. Construction equipment could temporarily obstruct or interfere with portions of residents views. The extent of obstruction could completely block particular views for short periods of time during the construction process. However, these impacts would be temporary. As such, construction within Area 1 has the potential to temporarily obstruct residents' views of the Pacific Ocean, a valued visual resource. This impact would be less than significant based on the short term duration.

No other sensitive viewer groups are located in close proximity to Area 1, and therefore views from the east, south, and west directions of Area 1 are not discussed in this analysis.

4.1 Aesthetics

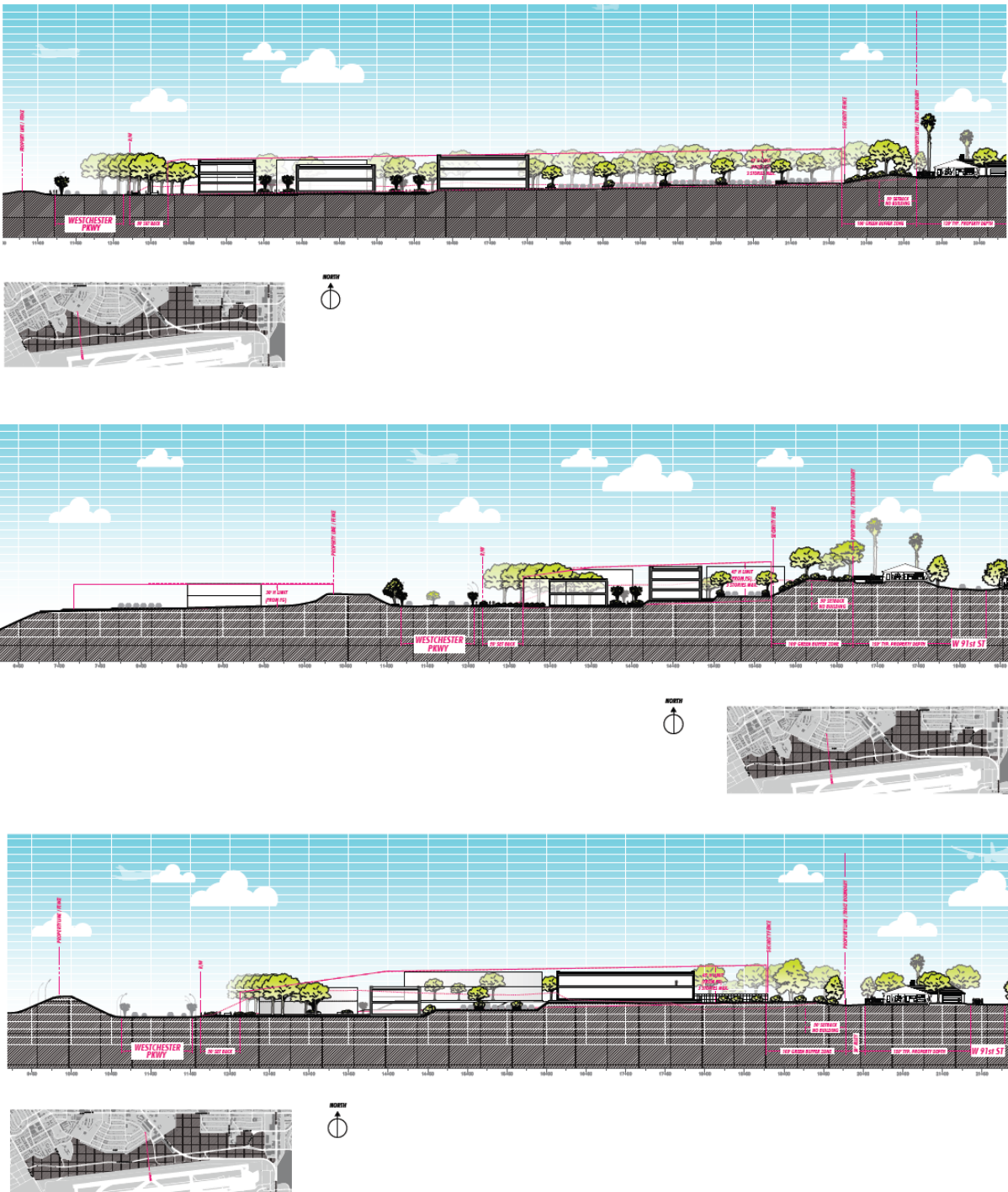
Area 2

Area 2 is bordered by residential development to the north, Loyola Boulevard to the east, Westchester Parkway to the south, and Falmouth Avenue to the west. Area 2 contains no documented views of valued scenic resources such as structures or natural landscapes.

Area 2 is designated for Office, Research and Development, Buffer, Community and Civic, and Recreation and Open Space uses. Grading in this area will decrease elevations for building frontages relative to existing residential development and provide a more accessible relationship with Westchester Parkway (**Figure 4.1-41**). Construction of structures to accommodate these uses would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 2 would be less than significant.

No other sensitive viewer groups are located in close proximity to Area 2, and therefore views from the east, south, and west directions of Area 2 are not evaluated in this analysis.

Figure 4.1-41 – Elevations of Area 2



Source: Rios Clementi Hale Studios, 2014

4.1 Aesthetics

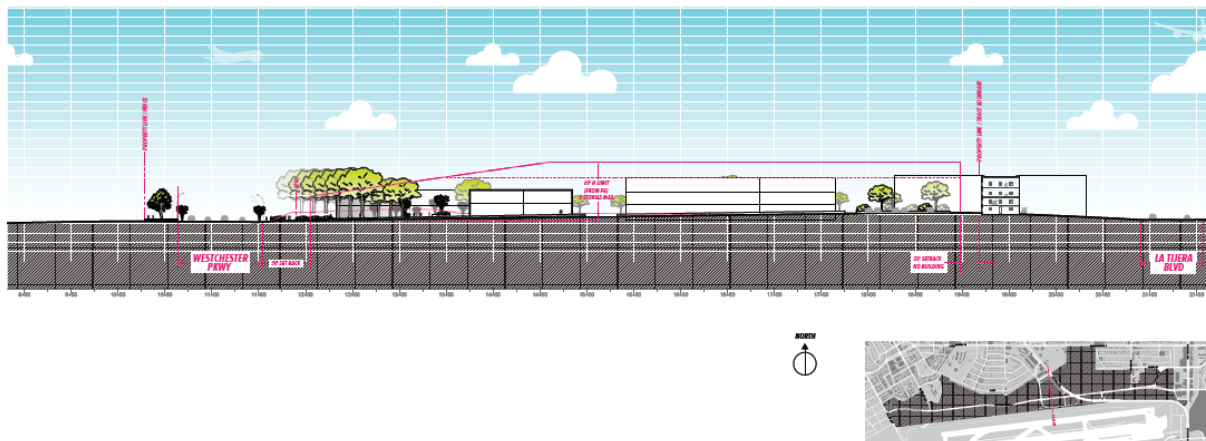
Area 3

Area 3 is bordered by multifamily residential development to the north, Lincoln Boulevard to the east, Westchester Parkway to the south, and Falmouth Avenue to the west. Area 3 contains no documented views of valued scenic resources such as structures or natural landscapes; however, the upper stories of the apartment complexes along Lincoln Boulevard may have views of the Pacific Ocean, which is a valued visual resource.

Area 3 is designated for Office, Research and Development uses. Building heights in Area 3 are restricted to no more than 60 feet. Grading in this area will decrease elevations for building frontages relative to existing residential development and provide a more accessible relationship with Westchester Parkway (**Figure 4.1-42**). Construction of structures to accommodate these uses could temporarily obstruct resident's views of the Pacific Ocean. However, the duration of this obstruction would be limited to the duration of construction, and is therefore considered less than significant.

No other sensitive viewer groups are located in close proximity to Area 3, and therefore views from the east, south, and west directions of Area 3 are not evaluated in this analysis.

Figure 4.1-42 – Elevation of Area 3



Source: Rios Clementi Hale Studios, 2014

LAX Northside Airport Support District

Area 4

Area 4 is bordered by the Westchester Parkway to the north, Loyola Boulevard to the east, the LAX North Runway Complex to the south, and Pershing Drive to the west. Area 4 contains no documented views of valued scenic resources such as structures or natural landscapes. Construction within Area 4 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 4 would be less than significant.

Area 5

Area 5 is bordered by the Westchester Parkway to the north, Lincoln Boulevard to the east, the Northside Parkway to the south, and Loyola Boulevard to the west. Area 5 contains no documented views of valued scenic resources such as structures or natural landscapes.

Construction within Area 5 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 5 would be less than significant.

Area 6

Area 6 is bordered by the Westchester Parkway to the north, Lincoln Boulevard to the east, the Northside Parkway to the south, and South McConnell Avenue to the west. Area 6 contains no documented views of valued scenic resources such as structures or natural landscapes. Construction within Area 6 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 6 would be less than significant.

Area 7

Area 7 is bordered by the Westchester Parkway to the north, Northside Parkway to the south, and McClean Parkway to the west. Area 7 contains no documented views of valued scenic resources such as structures or natural landscapes. Construction within Area 7 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 7 would be less than significant.

Area 8

Area 8 is bordered by the Westchester Parkway to the north, Lincoln Boulevard to the south and west, and S McConnell Ave to the west. Area 8 contains no documented views of valued scenic resources such as structures or natural landscapes. Construction within Area 8 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 8 would be less than significant.

Area 9

Area 9 is bordered by Westchester Parkway to the north, Sepulveda Westway to the east, Lincoln Boulevard to the south, and South McConnell Ave to the west. Area 9 contains no documented views of valued scenic resources such as structures or natural landscapes. Construction within Area 9 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 9 would be less than significant.

Area 10

Area 10 is bordered by Westchester Parkway to the north, Sepulveda Westway to the east, and Lincoln Boulevard to the west and south. Area 10 contains the Airport Landing Viewpoint. Buildings are prohibited within Area 10 as the entire Area is within the Limited Development Area. Therefore, no construction would occur within Area 10. As a result, there would no obstruction of valued views within Area 10.

Operation

LAX Northside Center District

Area 11

Area 11 is bordered by La Tijera Boulevard to the north and west, Sepulveda Westway to the east, and Westchester Parkway to the south. A single-family residential neighborhood abuts the north side of La Tijera. Residences are assumed to have high levels of potential sensitivity to changes in visual character. Existing views toward Area 11 from these residences are largely obstructed by a sequence of two soundwalls separated by La Tijera. The first soundwall abuts

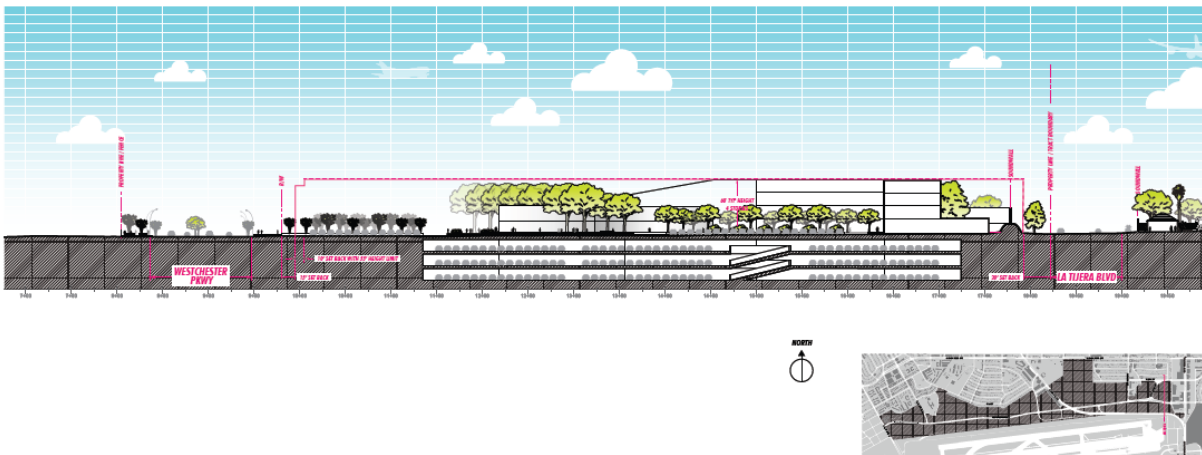
4.1 Aesthetics

the northern right-of-way of West 88th Street and La Tijera Drive. This soundwall separates the residential community from the public right of way. This soundwall would act as an initial obstruction, blocking the majority of structures beyond it. This is based on the principal that the elevation of this wall will impose a blockage of most structures in the background of the wall, except those that are so tall, their distance from these structures allows them to break the horizon line created by the wall.

Another soundwall is located on the north side of Area 11 which further obstructs any potential direct views of Area 11 from these residents. Therefore, while Area 11 is designated for Commercial Mixed Use land uses, building heights in Area 11 are restricted to no more than 60 feet and are prohibited within the Limited Development Area. Additionally, buildings are required to be set back 50 feet from the Limited Development Area, 30 feet from South La Tijera Avenue, and 15 feet from Sepulveda Avenue/La Tijera. Buildings located adjacent to the 88th Street and La Tijera property line are required to be stepped back by one foot for each additional foot of height above 15 feet (**Figure 4.1-43**). Therefore, operational activities within Area 11 are expected to be imperceptible to residents in the Community of Westchester because operations within Area 11 will not alter the existing design or location of the soundwalls, and the adoption of setback standards and design guidelines will not augment or obstruct the current views of these residents.

No other sensitive viewer groups are located in close proximity to Area 11, and therefore views from the east, south, and west directions of Area 11 are not discussed in this analysis.

Figure 4.1-43 – Elevation of Area 11



Source: Rios Clementi Hale Studios, 2014.

Area 12A East

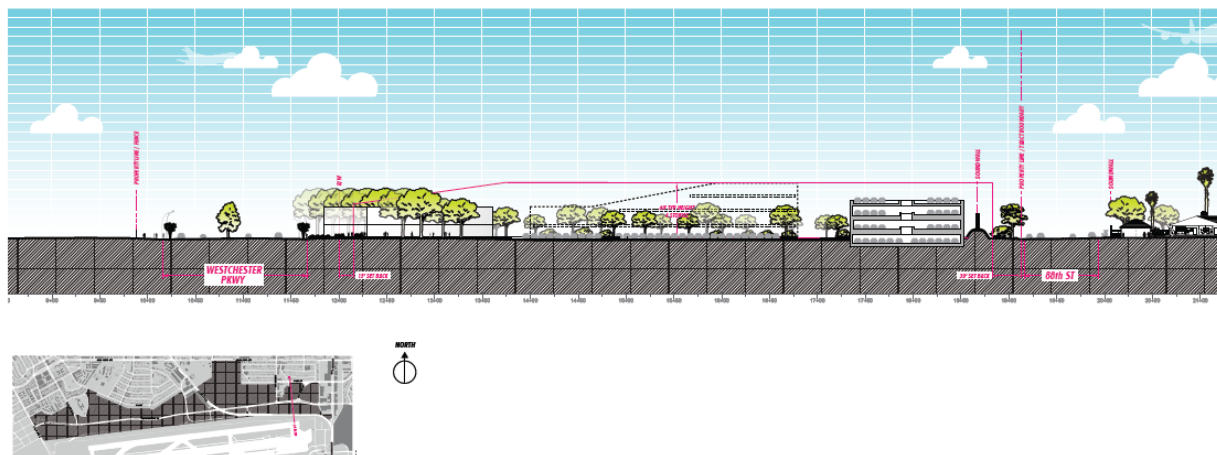
Area 12A East is bordered by La Tijera Boulevard to the north and west, Sepulveda Westway to the east, and Westchester Parkway to the south. A single-family residential neighborhood abuts the north side of La Tijera. Residences are assumed to have high levels of potential sensitivity to changes in visual character. Existing views toward Area 12A East from these residences are largely obstructed by a sequence of two soundwalls separated by La Tijera. The first soundwall abuts the northern right-of-way of West 88th Street and La Tijera Drive. This soundwall separates the residential community from the public right of way. This soundwall would act as an initial obstruction, blocking the majority of structures beyond it. This is based on the principal that the elevation of this wall will impose a blockage of most structures in the background of the

wall, except those that are so tall, that despite their distance from these residences, their height allows them to break the horizon line created by the wall.

Another soundwall is located on the north side of Area 12A East which further obstructs any potential direct views of Area 12A East from these residents. Therefore, while Area 12A East is designated for Commercial Mixed Use in the undeveloped portion and Community and Civic use in the portion currently developed with LAFD Fire Station Number 5, the potential building heights in Area 12A East would be restricted to no more than 60 feet, and would not be visible from the residential neighborhood north of W 88th Street (**Figure 4.1-44**). Furthermore, buildings are required to be set back 30 feet from West 88th Street, 18 feet from Westchester Parkway, 15 feet from La Tijera/West 88th Place, and 20 feet on the north and west edges of existing buildings. Additionally, buildings within Area 12A East are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback.

Therefore, operational activities within Area 12A East are expected to be imperceptible to residents in the Community of Westchester located north of W 88th Street because operations within Area 12A East will not alter the existing design or location of existing soundwalls. Further, the adoption of setback standards and design guidelines will ensure that current views are not obstructed.

Figure 4.1-44 – Elevation of Area 12A East



Source: Rios Clementi Hale Studios, 2014

Area 12A West

Area 12A West is bordered by Area 12B to the north, Emerson Ave to the east, Westchester Parkway to the south, and the Westchester Golf Course to the west. Area 12A West contains no documented views of valued scenic resources such as structures or natural landscapes. A single family neighborhood is located north of Area 12B and is separated from Area 12B by a soundwall and West 88th Street.

Residences are assumed to have high levels of potential sensitivity to changes in visual character. Existing views toward Area 12A West from these residences are largely obstructed by this soundwall because it acts as an initial obstruction, blocking the majority of views of structures beyond it. This is based on the principal that the elevation of this wall will impose a blockage of most structures in the background of the wall, except those that are so tall, that despite their distance from these residences, their height allows them to break the horizon line

4.1 Aesthetics

created by the wall. Therefore, while Area 12A West is designated for Community and Civic use, the heights of these buildings will be restricted to a maximum of 20 feet. Additionally, buildings in this Area will be required to be set back 30 feet from West 88th Street, 18 feet from Westchester Parkway, 15 feet from La Tijera/West 88th Place, and 20 feet on the north and west edges of existing buildings. Additionally, buildings within Area 12A West are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback. Therefore, operational activities within Area 12A West are expected to be imperceptible to residents in the Community of Westchester located north of W 88th Street because operations within Area 12A West will not alter the existing design or location of existing soundwalls. Further, the adoption of setback standards and design guidelines will ensure that residents' current views are not obstructed or negatively impacted.

Area 12B

Area 12B contains the Westchester Golf Course (**Figure 4.1-45**). No construction activities associated with the proposed Project will occur within the golf course area. Therefore, the proposed Project will not obstruct or interfere with existing views of Area 12B. There would be no impacts to Area 12B.

Figure 4.1-45 – Existing View of Area 12B, Westchester Golf Course



Source: URS Corporation, 2014

Area 13

Area 13 is bordered by Lincoln Boulevard to the west, the Westchester Golf Course to the east, and the Westchester Recreation Center to the north. Area 13 contains no documented views of valued scenic resources such as structures or natural landscapes. The existing First Flight Child Development Center would remain in its existing location.

Were future development to occur in Area 13, development would be subject to restrictions. Area 13 is designated for Community and Civic use. The existing First Flight Child Development Center would remain in its existing location and configuration. Building heights in Area 13 would be restricted to no more than 45 feet. Buildings would also be required to be set back 15 feet from Lincoln Boulevard, and 20 feet from the north and east edges of the Area. The provision of setbacks, height limits, and use restrictions will ensure that future development within Area 13 does not obstruct or interrupt existing views.

LAX Northside Campus District

Area 1

Area 1 is bordered by apartment complexes to the north, Falmouth Avenue to the east, Westchester Parkway to the south and Pershing Drive to the west. Area 1 contains no documented views of valued scenic resources such as structures or landscapes; however the upper stories of the apartment complexes to the north and northeast of Area 1 may have views of the Pacific Ocean, which is a valued visual resource.

Existing grading in Area 1 will remain the same, and is reserved for recreational type uses. As such construction of recreational uses such as soccer, lacrosse, baseball fields, or a dog park may occur. The long term operation of these facilities would require construction of a 10 foot fence around the field areas. Additionally, Area 1 also includes a 20 foot landscaping setback along the northeast property line of the Area. This landscaping buffer and fencing could obstruct residents' views of the Pacific Ocean. However, private views of valued resources are protected under CEQA. Therefore, while the proposed Project may allow development that permanently obstructs views of the Pacific Ocean from these residences, this not considered a significant impact.

No other sensitive viewer groups are located in close proximity to Area 1, and therefore views from nearby multifamily uses are the only resource considered.

Area 2

Area 2 is bordered by residential development to the north, Loyola Boulevard to the east, Westchester Parkway to the south, and Falmouth Avenue to the west. Area 2 contains no documented views of valued scenic resources such as structures or natural landscapes.

Area 2 is designated for Office, Research and Development, Buffer, Community and Civic, and Recreation and Open Space uses. Grading in this area will decrease elevations for building frontages relative to existing residential development and provide a more accessible relationship with Westchester Parkway. Building heights in Area 2 are restricted to no more than 45 feet. In subarea 2A, buildings are required to be set back 15 feet from St. Bernard/West 91st Street/South Cum Laude Avenue, 20 feet from West Cum Laude Avenue and the eastern edge of the subarea, 30 feet from Falmouth Avenue, and 38 feet from Westchester Parkway. In subareas 2C and 2D, buildings are required to be set back 20 feet from the north and west edges of the subareas and 38 feet from Westchester Parkway. In subarea 2E, buildings are required to be set back 15 feet from Loyola Boulevard, 20 feet from the north and west edges of the subarea, and 38 feet from Westchester Parkway. Subarea 2B is designated as a Buffer use that extends along the north of subareas 2C, 2D, and 2E and directly south of existing residential uses. Buildings are prohibited in this 100-foot wide area. The Buffer is required to be secured by a fence that matches landscaping. Additionally, buildings within Area 2 are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback. Buildings within subareas 2C and 2E are required to be located with a minimum of 65 percent of the proposed Project ground floor building square footage within 250 feet of the Westchester Parkway property line. Grading in this area will decrease elevations for building frontages relative to existing residential development and provide a more accessible relationship with Westchester Parkway.

The proposed Project would not obstruct, interrupt, or diminish views of valued scenic resources. Therefore, the proposed Project would have less than significant impacts in Area 2.

4.1 Aesthetics

Area 3

Area 3 is bordered by residential development to the north, Lincoln Boulevard to the east, Westchester Parkway to the south, and Falmouth Avenue to the west. Area 3 contains no documented views of valued scenic resources such as structures or natural landscapes, however, the upper stories of the apartment complexes along Lincoln Boulevard may have views of the Pacific Ocean, which is a valued visual resource.

Area 3 is designated for Office, Research and Development uses. Building heights in Area 3 are restricted to no more than 60 feet. Buildings are required to be set back 15 feet from Loyola Boulevard, 20 feet from the north and west edges of the area, and 38 feet from Westchester Parkway. Grading in this area will decrease elevations for building frontages relative to existing residential development and provide a more accessible relationship with Westchester Parkway.

Construction of new structures would be capped at 60 feet, blocking residents within the multifamily building views of the Pacific Ocean. However, views from private residences are not protected and therefore the proposed Project would have less than significant impacts in Area 3.

LAX Northside Airport Support District

Area 4

Area 4 is bordered by the Westchester Parkway to the north, Loyola Boulevard to the east, the LAX North Runway Complex to the south, and Pershing Drive to the west. Area 4 contains no documented views of valued scenic resources such as structures or natural landscapes. Buildings are required to be set back 50 feet from South Pershing Drive and Westchester Parkway, 20 feet from the southern edge of the Area, and 15 feet from Northside Parkway. As such, structures within Area 4 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 4 would be less than significant.

Area 5

Area 5 is bordered by the Westchester Parkway to the north, Lincoln Boulevard to the east, the Northside Parkway to the south, and Loyola Boulevard to the west. Area 5 contains no documented views of valued scenic resources such as structures or natural landscapes. Buildings are required to be set back 50 feet from Westchester Parkway and 15 feet from Northside Parkway/Georgetown Avenue/McClean Parkway. As such, structures within Area 5 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 5 would be less than significant.

Area 6

Area 6 is bordered by the Westchester Parkway to the north, Lincoln Boulevard to the east, the Northside Parkway to the south, and South McConnell Avenue to the west. Area 6 contains no documented views of valued scenic resources such as structures or natural landscapes. Buildings are required to be set back 50 feet from Westchester Parkway and 15 feet from Northside Parkway/Georgetown Avenue/McClean Parkway. As such, structures within Area 6 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 6 would be less than significant.

Area 7

Area 7 is bordered by the Westchester Parkway to the north, Northside Parkway to the south, and McClean Parkway to the west. Area 7 contains no documented views of valued scenic resources such as structures or natural landscapes. Buildings are required to be set back 15

feet from Lincoln Boulevard/McClean Parkway. As such, structures within Area 7 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 7 would be less than significant.

Area 8

Area 8 is bordered by the Westchester Parkway to the north, Lincoln Boulevard to the south and west, and S McConnell Ave to the west. Area 8 contains no documented views of valued scenic resources such as structures or natural landscapes. Buildings are required to be set back 15 feet from all edges of the Area. Construction within Area 8 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 8 would be less than significant.

Area 9

Area 9 is bordered by Westchester Parkway to the north, Sepulveda Westway to the east, Lincoln Boulevard to the south, and South McConnell Ave to the west. Area 9 contains no documented views of valued scenic resources such as structures or natural landscapes. Buildings are required to be set back 15 feet from Westchester Parkway and South McConnell Avenue. Construction within Area 9 would not interfere, obstruct, or interrupt views of valued scenic resources. Therefore, impacts to documented views of valued scenic resources within Area 9 would be less than significant.

Area 10

Area 10 is bordered by Westchester Parkway to the north, Sepulveda Westway to the east, and Lincoln Boulevard to the west and south. Area 10 contains the Airport Landing Viewpoint. Buildings are prohibited within Area 10 as the entire Area is within the Limited Development Area. Therefore, no construction would occur within Area 10. As a result, there would no obstruction of valued views within Area 10.

4.1.3.5 Light and Glare Impacts

4.1.3.5.1 Change in Ambient Illumination Levels

Construction

Proposed Project construction activities include grading, clearing, and grubbing the land; installing utilities, building foundations, superstructures, and building skin/roofing; completing interior framing and finishing; installing hardscape and landscaping; and building testing/commissioning. Construction equipment includes, but is not limited to drill rigs, cement and mortar mixers, forklifts, graders, cranes, and tractors. Construction related lighting would include backup lights on vehicles, perimeter lighting, and safety lighting. Construction equipment would not include large expanses of mirror or reflective surfaces that could cause glare impacts. Construction activities would comply with LAMC Section 41.40, which limits the hours of construction between 7:00 a.m. and 9:00 p.m. on weekdays and between 8:00 a.m. and 6:00 p.m. on Saturdays, with no construction permitted on Sundays. Furthermore, construction-related illumination would be used for safety and security purposes only, in compliance with LAMC light intensity requirements. Construction activities would be temporary and occur mainly during daylight hours. Impacts specific to each District and each Area are discussed below.

4.1 Aesthetics

LAX Northside Center District

Area 11

Area 11 lighting includes perimeter street lights to the south and west along Westchester Parkway and La Tijera as well as lights associated with temporary construction bungalows and vehicles. Light-sensitive uses include residences located directly to the north across La Tijera. Residences are separated from Area 11 by private walls, La Tijera, landscaping along the south side of La Tijera, and the soundwall along the northern edge of Area 11. Construction of buildings on Area 11 is required to be set back 15 feet from Sepulveda Avenue/La Tijera. Additionally, building construction within Area 11 is required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback. Construction lighting would comply with LAMC hour restrictions, would be shielded from light-sensitive uses by the existing soundwall, and would take place away from light-sensitive uses per the proposed Project's setback and building location requirements. Therefore, construction lighting and glare impacts on Area 11 would be less than significant.

Area 12A East

Area 12A East lighting includes perimeter street lights to the north, south, and east along 88th Place and 88th Street, Westchester Parkway and La Tijera and existing parking lot and exterior lighting for the LAFD Fire Station Number 5. Light-sensitive uses include residences located directly to the north across 88th Street and the Westchester-Emerson Adult School located directly to the north along 88th Place. Residences are separated from Area 12A East by private walls, 88th Street, landscaping along the south side of 88th Street, and the soundwall along the northern edge of Area 12A East. Construction activities would not take place at the site of the existing LAFD Fire Station Number 5, located directly south of the Westchester-Emerson Adult School. Construction of buildings on Area 12A East is required to be set back 30 feet from West 88th Street and 15 feet from La Tijera/West 88th Place. Additionally, building construction within Area 12A East is required to be located adjacent to the Westchester Parkway and La Tijera setback. Construction lighting would comply with LAMC hour restrictions, would be shielded from light-sensitive uses by the existing soundwall, and would take place away from light-sensitive uses per the proposed Project's setback and building location requirements. Therefore, construction lighting and glare impacts on Area 12A East would be less than significant.

Area 12A West

Area 12A West lighting includes street lighting along Westchester Parkway to the south. Area 12A West does not have adjacent light-sensitive uses as Westchester Parkway and airport support uses are located to the south, the heavily illuminated Westchester Golf Course is located to the west and north, and LAFD Station Number 5 and vacant Area 12A East are located to the east. Buildings within Area 12A West are required to be located adjacent to the Westchester Parkway setback. Construction lighting would comply with LAMC hour restrictions, would not take place near light sensitive uses, and would be concentrated along Westchester Parkway per the proposed Project's setback and building location requirements. Therefore, construction lighting and glare impacts on Area 12A West would be less than significant.

Area 12B

Area 12B lighting includes lighting associated with the existing Westchester Golf Course and clubhouse. Flood lights are present on the golf course, while the clubhouse and parking lot contain buildings and street lights. Existing lighting would remain in this area in its existing

location and configuration and no construction activities would occur in Area 12B. Therefore, no construction lighting and glare impacts would occur on Area 12B.

Area 13

Area 13 lighting includes street lights along Lincoln Boulevard and parking lot, building and driveway lighting for the existing First Flight Child Development Center. Existing lighting would remain in its existing location and configuration. Adjacent light-sensitive uses include multi-family residences to the south and west. These uses are separated from Area 13 by Lincoln Boulevard, which is an existing, well lit corridor. Additionally, building construction on Area 13 would be set back 15 feet from Lincoln Boulevard. The Westchester Golf Course to the east and First Flight Child Development Center to the north are not light-sensitive uses because neither requires minimal nighttime illumination for proper function, physical comfort, or commerce. Construction lighting would comply with LAMC hour restrictions, would not take place near light sensitive uses, and would be set back an additional 15 feet per the proposed Project's setback requirements. Therefore, construction lighting and glare impacts on Area 13 would be less than significant.

LAX Northside Campus District

Area 1

Area 1 lighting includes street lights along Falmouth Avenue, Westchester Parkway, and Pershing Drive as well as exterior structure lights at the Jet Pets animal quarantine facility. Existing lighting associated with Jet Pets would remain in its existing location and configuration and construction activities would not take place on the portion of Area 1 that is currently occupied by Jet Pets. Adjacent light-sensitive uses include multi-family residences to the north and open space to the west. The Saint Bernard High School football field and parking lots located to the east contain flood lights and are not light-sensitive uses because they already use nighttime lighting and do not require minimal illumination for proper function, physical comfort, or commerce. A 20 foot wide Buffer is designated along the northern edge of Area 1 that would separate construction activities from adjacent residences. Additionally, building construction is required to be set back 80 feet from the Buffer, for a total 100 feet of separation of building construction from existing residences. No construction activities would take place on the western edge of Area 1 adjacent to Pershing Drive and existing open space. Construction lighting would comply with LAMC hour restrictions, would not take place near light sensitive open space, and building construction would be separated from adjacent uses by a minimum 100 feet. Therefore, construction lighting and glare impacts on Area 1 would be less than significant.

Area 2

Area 2 lighting includes street lights along Falmouth Avenue, West 92nd Street, Cum Laude Avenue, Westchester Parkway, and Loyola Boulevard. Adjacent light-sensitive uses include single-family residences to the north and multi-family residences to the east. The Saint Bernard High School baseball field and parking lots located to the west contain flood lights and are not light-sensitive uses because they already use nighttime lighting and do not require minimal illumination for proper function, physical comfort, or commerce. The Otis College parking structure to the east is not a light-sensitive use as it also contains illumination and does not require minimal illumination for proper function. The proposed Project requires a 100-foot Buffer along the northern edge of Area 2 adjacent to residences and a 20-foot setback from the Buffer where building construction is prohibited. Additionally, building construction within Area 2 is required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway

4.1 Aesthetics

setback. Construction lighting would comply with LAMC hour restrictions and building construction would be separated from adjacent residential uses by a minimum 120 feet. Therefore, construction lighting and glare impacts on Area 2 would be less than significant.

Area 3

Area 3 lighting includes street lights along Loyola Boulevard and Westchester Parkway. Adjacent light-sensitive uses include multi-family residences to the north and east. Building construction is required to be set back 20 feet from the north and east edges of the Area. Construction activity lighting would be visible from balconies of multi-family residences that face Area 3, however construction lighting would be temporary, would comply with LAMC hour restrictions, and building construction would be separated from adjacent residential uses by a minimum 20 feet. Therefore, construction lighting and glare impacts on Area 3 would be less than significant.

LAX Northside Airport Support District

Area 4

Area 4 is substantially illuminated. Lighting on Area 4 includes street lighting along Pershing Drive and Westchester Parkway, perimeter lighting around existing airport support and construction laydown areas, and parking lot lighting. Open space located to the west across Pershing Drive is the only adjacent light-sensitive use. Building construction in Area 4 is required to be set back 50 feet from South Pershing Drive. Construction lighting would comply with LAMC hour restrictions and building construction would be separated from adjacent open space uses by a minimum 50 feet. Therefore, construction lighting and glare impacts on Area 4 would be less than significant.

Area 5

Area 5 lighting includes street lights along Westchester Parkway, Northside Parkway, and Georgetown Avenue. There are no light-sensitive uses adjacent to Area 5. Construction lighting would comply with LAMC hour restrictions. Therefore, construction lighting and glare impacts on Area 5 would be less than significant.

Area 6

Area 6 lighting includes street lights along Westchester Parkway, Northside Parkway, and Georgetown Avenue. There are no light-sensitive uses adjacent to Area 6. Construction lighting would comply with LAMC hour restrictions. Therefore, construction lighting and glare impacts on Area 6 would be less than significant.

Area 7

Area 7 lighting includes street lights along Lincoln Boulevard and Northside Parkway. There are no light-sensitive uses adjacent to Area 7. Construction lighting would comply with LAMC hour restrictions. Therefore, construction lighting and glare impacts on Area 7 would be less than significant.

Area 8

Area 8 lighting includes street lights along Lincoln Boulevard and Westchester Parkway. There are no light-sensitive uses adjacent to Area 8. The Westchester Golf Course located to the north across Westchester Parkway is already substantially illuminated with flood lights and is not a light-sensitive use as it does not require minimal illumination for proper function. Construction lighting would comply with LAMC hour restrictions. Therefore, construction lighting and glare impacts on Area 8 would be less than significant.

Area 9

Area 9 lighting includes street lights along Westchester Parkway, Lincoln Boulevard, and Sepulveda Westway and structure lighting for the LAX radar facility. There are no light-sensitive uses adjacent to Area 9. The adjacent commercial parking structure located to the east along Sepulveda Westway is not a light-sensitive use as it is already substantially illuminated and does not require minimal lighting for proper function, physical comfort, or commerce. Construction lighting would comply with LAMC hour restrictions. Therefore, construction lighting and glare impacts on Area 9 would be less than significant.

Area 10

Area 10 lighting includes street lighting along Lincoln Boulevard, West 92nd Street, and Sepulveda Boulevard. Building construction is prohibited within Area 10 as the entire Area is within the Limited Development Area. Therefore, construction lighting and glare impacts would not occur on Area 10.

Operation

Operation of the proposed Project would introduce new lighting on the Project site to increase visibility, promote safety, and enhance the nighttime environment. Proposed Project light sources would consist of exterior lighting along pedestrian walkways and the Paseo, vehicle driveways, and parking lots, as well as lighting for signage, security, architectural, and landscaping purposes.

Existing street lights would remain, while new street lights would be coordinated with the City of Los Angeles Bureau of Street Lighting to maintain appropriate and safe lighting on sidewalks and roadways, while minimizing light and glare on adjacent properties. Proposed Project lighting would comply with all applicable LAMC lighting standards. Outdoor lights for parking areas would be designed to reflect light away from adjacent streets or property. Exterior light would be designed to not generate a light intensity greater than two foot-candles onto specified off-site habitable or recreational uses. Further, signage illumination would be limited to a light intensity of three foot-candles above ambient lighting, as measured at the property line of the nearest residentially-zoned property.

Additionally, the proposed Project Design Features control new lighting that may be introduced with the proposed Project. The following Project Design Features apply to ambient illumination:

- Lighting is required to be designed to provide ambiance, safety, and security without unnecessary spillover or glare onto adjacent properties.
- Indirect wall lighting or “wall washing” and overhead down lighting may be used to help reduce light trespass into adjacent properties.
- Spotlighting or glare from any site lighting is required to be shielded from adjacent properties and directed at a specific object or target area.
- Exposed bulbs are prohibited.
- When security lighting is necessary, it is required to be recessed, hooded, and located to illuminate only the intended area.
- Off-site glare and light trespass shall be prevented.
- Glare or light trespass is prohibited on any adjacent streets, or within any adjacent properties.

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- Lighting mounted above ten (10) feet from finish grade shall incorporate a full cut-off shield fixture.
- Service area lighting shall be contained within the service yard boundaries and enclosure walls.
- No light spillover shall occur outside the service area.
- Spotlighting or glare from any site lighting shall be shielded from adjacent properties and directed at a specific object or target area.
- Buildings within 150 feet of residences shall be located to maximize privacy through building orientation or off-setting windows on any walls facing a residence to prevent direct views and light spillover into any neighboring windows.
- Mirror or reflective surfaces are prohibited as primary building materials.

Impacts specific to each District and each Area are discussed below.

LAX Northside Center District

Area 11

Area 11 lighting includes perimeter street lights to the south and west along Westchester Parkway and La Tijera as well as lights associated with temporary construction bungalows and vehicles. This existing lighting would remain and operations lighting would include new lighting for parking lots, the Paseo, pedestrian walkways, and exterior lighting for Commercial Mixed Use land uses. Light-sensitive uses include residences located directly to the north across La Tijera. Residences are separated from Area 11 by private walls, La Tijera, landscaping along the south side of La Tijera, and the soundwall along the northern edge of Area 11. Buildings on Area 11 are required to be set back 15 feet from Sepulveda Avenue/La Tijera. Additionally, buildings within Area 11 are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback and away from existing light-sensitive residences to the north. The existing soundwall and landscaping north of the soundwall would remain. Windows facing residences would be designed to maintain privacy and avoid light spillover. Buildings located adjacent to the 88th Street and La Tijera property line are required to be stepped back by one foot for each additional foot of height above 15 feet, creating a further distance between new light sources and existing residences. Operations lighting in Area 11 would be shielded from light-sensitive uses by the existing soundwall and would take place away from light-sensitive uses per the proposed Project's setback, stepback, and building location requirements (**Figure 4.1-46**). The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime lighting levels, and would not interfere with nearby sensitive uses. Therefore, operational lighting and glare impacts on Area 11 would be less than significant.

Figure 4.1-46 – Proposed Lighting Elevation and Plan for Area 11



Source: Rios Clementi Hale Studios, 2014

Area 12A East

Area 12A East lighting includes perimeter street lights to the north, south, and east along 88th Place and 88th Street, Westchester Parkway and La Tijera and existing parking lot and exterior lighting for the LAFD Fire Station Number 5. This existing lighting would remain and operations lighting would include new lighting for parking lots, the Paseo, pedestrian walkways, and exterior lighting for Commercial Mixed Use land uses. Light-sensitive uses include residences located directly to the north across 88th Street and the Westchester-Emerson Adult School located directly to the north along 88th Place. Residences are separated from Area 12A East by private walls, 88th Street, landscaping along the south side of 88th Street, and the soundwall along the northern edge of Area 12A East. Operation lighting at the LAFD Station Number 5, located directly south of the Westchester-Emerson Adult School, would not change. Buildings on Area 12A East are required to be set back 30 feet from West 88th Street and 15 feet from La Tijera/West 88th Place. Additionally, buildings within Area 11 are required to be located adjacent to the Westchester Parkway and La Tijera setback. Construction lighting would comply with LAMC hour restrictions, would be shielded from light-sensitive uses by the existing soundwall, and would take place away from light-sensitive uses per the proposed Project's setback and building location requirements. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime lighting levels, and would not interfere with nearby sensitive uses. Therefore, operational lighting and glare impacts on Area 12A East would be less than significant.

Area 12A West

Area 12A West lighting includes street lighting along Westchester Parkway to the south. This existing lighting would remain and operations lighting would include new lighting for parking lots, the Paseo, pedestrian walkways, and exterior lighting for Civic and Community land uses. Area 12A West does not have adjacent light-sensitive uses as Westchester Parkway and airport support uses are located to the south, the heavily illuminated. Westchester Golf Course is located to the west and north, and LAFD Station Number 5 and vacant Area 12A East are located to the east. Buildings within Area 12A West are required to be located adjacent to the Westchester Parkway setback. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime

4.1 Aesthetics

lighting levels, and would not interfere with nearby sensitive uses. Therefore, operational lighting and glare impacts on Area 12A West would be less than significant.

Area 12B

Area 12B lighting includes lighting associated with the existing Westchester Golf Course and clubhouse. Flood lights are present on the golf course, while the clubhouse and parking lot contain building and street lights. Existing lighting would remain in this area in its existing location and configuration and no changes in operation would occur in Area 12B under the proposed Project. Therefore, no operations lighting and glare impacts would occur on Area 12B.

Area 13

Area 13 lighting includes street lights along Lincoln Boulevard and parking lot, building, and driveway lighting for the existing First Flight Child Development Center. Existing lighting would remain in its existing location and configuration. Operations lighting would include new lighting for parking lots, pedestrian walkways, and exterior lighting for Civic and Community land uses. Operational lighting associated with the First Flight Child Development Center would not change under the proposed Project. Adjacent light-sensitive uses include multi-family residences to the south and west. These uses are separated from Area 13 by Lincoln Boulevard, which is an existing, well lit corridor. Additionally, buildings on Area 13 would be set back 15 feet from Lincoln Boulevard. The Westchester Golf Course to the east and First Flight Child Development Center to the north are not light-sensitive uses because neither requires minimal nighttime illumination for proper function, physical comfort, or commerce. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime lighting levels, and would not interfere with nearby sensitive uses. Therefore, operational lighting and glare impacts on Area 13 would be less than significant.

LAX Northside Campus District

Area 1

Area 1 lighting includes street lights along Falmouth Avenue, Westchester Parkway, and Pershing Drive as well as exterior structure lights at the Jet Pets animal quarantine facility. Existing lighting associated with Jet Pets would remain in its existing location and configuration and would not change with the proposed Project. Operations lighting would include new lighting for parking lots, the Paseo, pedestrian walkways, and exterior lighting for Office, Research and Development or Open Space land uses. Adjacent light-sensitive uses include multi-family residences to the north and open space to the west. The Saint Bernard High School football field and parking lots located to the east contain flood lights and are not light-sensitive uses because they already use nighttime lighting and do not require minimal illumination for proper function, physical comfort, or commerce. A 20-foot wide Buffer is designated along the northern edge of Area 1 that would separate and screen operational activities from adjacent residences. Additionally, buildings are required to be set back 80 feet from the Buffer, for a total 100 feet of separation of buildings from existing residences. No changes to operations lighting would take place on the western edge of Area 1 adjacent to Pershing Drive and existing open space. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime lighting levels, and would not interfere with nearby sensitive uses. Therefore, operational lighting and glare impacts on Area 1 would be less than significant.

Area 2

Area 2 lighting includes street lights along Falmouth Avenue, West 92nd Street, Cum Laude Avenue, Westchester Parkway, and Loyola Boulevard. Operations lighting would include new lighting for parking lots, the Paseo, pedestrian walkways, and exterior lighting for Office, Research and Development, Open Space, and Community and Civic land uses. Adjacent light-sensitive uses include single-family residences to the north and multi-family residences to the east. The Saint Bernard High School baseball field and parking lots located to the west contain flood lights and are not light-sensitive uses because they already use nighttime lighting and do not require minimal illumination for proper function, physical comfort, or commerce. The Otis College parking structure to the east is not a light-sensitive use as it also contains illumination and does not require minimal illumination for proper function. The proposed Project requires a 100-foot Buffer along the northern edge of Area 2 adjacent to residences and a 20 foot setback from the Buffer where buildings are prohibited. Additionally, buildings within Area 2 are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setbacks. Operational lighting may be visible from the backyards of some directly adjacent residential uses to the north. However, new lighting would be separated from residences by the Buffer and setbacks, would be shielded with new landscaping, and would be designed to be shielded and minimize impacts on ambient illumination (**Figure 4.1-47**). The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime lighting levels, and would not interfere with nearby sensitive uses. Therefore, operational lighting and glare impacts on Area 2 would be less than significant.

Figure 4.1-47 – Proposed Lighting Elevation and Plan for Area 2



Source: Rios Clementi Hale Studios, 2014

Area 3

Area 3 lighting includes street lights along Loyola Boulevard and Westchester Parkway. Operations lighting would include new lighting for parking lots, pedestrian walkways, and exterior lighting for Office, Research and Development land uses. Adjacent light-sensitive uses include multi-family residences to the north and east. Buildings are required to be set back 20 feet from the north and east edges of the Area. Operational lighting may be visible from the balconies of some directly adjacent residential uses to the north and east. However, new lighting would be separated from residences by the setbacks, would be similar in nature to existing street lighting, and would be designed to be shielded and minimize impacts on ambient illumination. The proposed Project's lighting sources would comply with LAMC requirements

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and the Project Design Features, would not significantly increase nighttime lighting levels, and would not interfere with nearby sensitive uses. Therefore, operational lighting and glare impacts on Area 3 would be less than significant.

LAX Northside Airport Support District

Area 4

Area 4 is substantially illuminated. Lighting on Area 4 includes street lighting along Pershing Drive and Westchester Parkway, perimeter lighting around existing airport support and construction laydown areas, and parking lot lighting. Operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses. Open space located to the west across Pershing Drive is the only adjacent light-sensitive use. Buildings in Area 4 are required to be set back 50 feet from South Pershing Drive. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime lighting levels, and would be separated from sensitive open space uses to the west by Pershing Drive and the 50 foot setback. Therefore, operational lighting and glare impacts on Area 4 would be less than significant.

Area 5

Area 5 lighting includes street lights along Westchester Parkway, Northside Parkway, and Georgetown Avenue. There are no light-sensitive uses adjacent to Area 5. Operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime lighting levels, and would be similar to existing lighting at adjacent LAX land uses. Therefore, operational lighting and glare impacts on Area 5 would be less than significant.

Area 6

Area 6 lighting includes street lights along Westchester Parkway, Northside Parkway, and Georgetown Avenue. There are no light-sensitive uses adjacent to Area 6. Operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime lighting levels, and would be similar to existing lighting at adjacent LAX land uses. Therefore, operational lighting and glare impacts on Area 6 would be less than significant.

Area 7

Area 7 lighting includes street lights along Lincoln Boulevard and Northside Parkway. There are no light-sensitive uses adjacent to Area 7. Operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime lighting levels, and would be similar to existing lighting at adjacent LAX land uses. Therefore, operational lighting and glare impacts on Area 7 would be less than significant.

Area 8

Area 8 lighting includes street lights along Lincoln Boulevard and Westchester Parkway. There are no light-sensitive uses adjacent to Area 8. The Westchester Golf Course located to the north across Westchester Parkway is already substantially illuminated with flood lights and is not a light-sensitive use as it does not require minimal illumination for proper function. Operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses.

The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime lighting levels, and would be similar to existing lighting at adjacent LAX land uses. Therefore, operational lighting and glare impacts on Area 8 would be less than significant.

Area 9

Area 9 lighting includes street lights along Westchester Parkway, Lincoln Boulevard, and Sepulveda Westway and structure lighting for the LAX radar facility. There are no light-sensitive uses adjacent to Area 9. The adjacent commercial parking structure located to the east along Sepulveda Westway is not a light-sensitive use as it is already substantially illuminated and does not require minimal lighting for proper function, physical comfort, or commerce. Operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would not significantly increase nighttime lighting levels, and would be similar to existing lighting at adjacent LAX land uses. Therefore, operational lighting and glare impacts on Area 9 would be less than significant.

Area 10

Area 10 lighting includes street lighting along Lincoln Boulevard, West 92nd Street, and Sepulveda Boulevard. Building construction is prohibited within Area 10 as the entire Area is within the Limited Development Area. Therefore, operations lighting and glare impacts would not occur on Area 10.

Light Spillover onto Sensitive Areas

Construction

Proposed Project construction activities include grading, clearing, and grubbing the land; installing utilities, building foundations, superstructures, and building skin/roofing; completing interior framing and finishing; installing hardscape and landscaping; and building testing/commissioning. Construction equipment includes, but is not limited to drill rigs, cement and mortar mixers, forklifts, graders, cranes, , and tractors. Construction related lighting would include backup lights on vehicles, perimeter lighting, and safety lighting. Construction activities would comply with LAMC Section 41.40, which limits the hours of construction between 7:00 a.m. and 9:00 p.m. on weekdays and between 8:00 a.m. and 6:00 p.m. on Saturdays, with no construction permitted on Sundays. Furthermore, construction-related illumination would be used for safety and security purposes only, in compliance with LAMC light intensity requirements. Construction activities would be temporary and occur mainly during daylight hours. Impacts specific to each District and each Area are discussed below.

LAX Northside Center District

Area 11

Light-sensitive uses adjacent to Area 11 include residences located directly to the north across La Tijera. Residences are separated from Area 11 by private walls, La Tijera, landscaping along the south side of La Tijera, and the soundwall along the northern edge of Area 11. Construction of buildings on Area 11 is required to be set back 15 feet from Sepulveda Avenue/La Tijera. Additionally, building construction within Area 11 is required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback. The existing soundwall and Project Design Features would prevent construction light from spilling over onto sensitive uses. Therefore, construction light spillover impacts on Area 11 would be less than significant.

4.1 Aesthetics

Area 12A East

Light-sensitive uses adjacent to Area 12A East include residences located directly to the north across 88th Street and the Westchester-Emerson Adult School located directly to the north along 88th Place. Residences are separated from Area 12A East by private walls, 88th Street, landscaping along the south side of 88th Street, and the soundwall along the northern edge of Area 12A East. Construction activities would not take place at the site of the existing LAFD Fire Station Number 5, located directly south of the Westchester-Emerson Adult School. Construction of buildings on Area 12A East is required to be set back 30 feet from West 88th Street and 15 feet from La Tijera/West 88th Place. Additionally, building construction within Area 12A East is required to be located adjacent to the Westchester Parkway and La Tijera setback. The existing soundwall and Project Design Features would prevent construction light from spilling over onto sensitive uses. Therefore, construction light spillover impacts on Area 12A East would be less than significant.

Area 12A West

Area 12A West does not have adjacent light-sensitive uses as Westchester Parkway and airport support uses are located to the south, the heavily illuminated Westchester Golf Course is located to the west and north, and LAFD Station Number 5 and vacant Area 12A East are located to the east. Construction light would not spill over onto sensitive uses as there are no sensitive uses located adjacent to Area 12A West. Therefore, no construction light spillover impacts on Area 12A West would occur.

Area 12B

The proposed Project does not include any new lighting for the Westchester Golf Course. Therefore, no construction light spillover impacts on Area 12B would occur.

Area 13

Light-sensitive uses adjacent to Area 13 include multi-family residences to the south and west. These uses are separated from Area 13 by Lincoln Boulevard, which is an existing, well lit corridor. Additionally, building construction on Area 13 would be set back 15 feet from Lincoln Boulevard. The Westchester Golf Course to the east and First Flight Child Development Center to the north are not light-sensitive uses because neither requires minimal nighttime illumination for proper function, physical comfort, or commerce. The existing Lincoln Boulevard and Project Design Features would prevent construction light from spilling over onto sensitive uses. Therefore, construction light spillover impacts on Area 13 would be less than significant.

LAX Northside Campus District

Area 1

Light-sensitive uses adjacent to Area 1 include multi-family residences to the north and open space to the west. The Saint Bernard High School football field and parking lots located to the east contain flood lights and are not light-sensitive uses because they already use nighttime lighting and do not require minimal illumination for proper function, physical comfort, or commerce. Construction activities would not take place on the portion of Area 1 that is currently occupied by Jet Pets. A 20 foot wide Buffer is designated along the northern edge of Area 1 that would separate construction activities from adjacent residences. Additionally, building construction is required to be set back 80 feet from the Buffer, for a total 100 feet of separation of building construction from existing residences. No construction activities would take place on the western edge of Area 1 adjacent to Pershing Drive and existing open space. The lack of construction activity adjacent to open space and residences west and north of Jet Pets and

Project Design Features would prevent construction light from spilling over onto sensitive uses. Therefore, construction light spillover impacts on Area 1 would be less than significant.

Area 2

Light-sensitive uses adjacent to Area 2 include single-family residences to the north and multi-family residences to the east. The Saint Bernard High School baseball field and parking lots located to the west contain flood lights and are not light-sensitive uses because they already use nighttime lighting and do not require minimal illumination for proper function, physical comfort, or commerce. The Otis College parking structure to the east is not a light-sensitive use as it also contains illumination and does not require minimal illumination for proper function. The proposed Project requires a 100-foot Buffer along the northern edge of Area 2 adjacent to residences and a 20 foot setback from the Buffer where building construction is prohibited. Additionally, building construction within Area 2 is required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback. Project Design Features would prevent construction light from spilling over onto sensitive uses. Therefore, construction light spillover impacts on Area 2 would be less than significant.

Area 3

Light-sensitive uses adjacent to Area 3 include multi-family residences to the north and east. Building construction is required to be set back 20 feet from the north and east edges of the Area. Construction activity lighting would be visible from balconies of multi-family residences that face Area 3, however construction lighting would be temporary, would comply with LAMC hour restrictions, and building construction would be separated from adjacent residential uses by a minimum 20 feet. Therefore, construction lighting and glare impacts on Area 3 would be less than significant.

LAX Northside Airport Support District

Area 4

Open space located to the west across Pershing Drive is the only adjacent light-sensitive use. Building construction in Area 4 is required to be set back 50 feet from South Pershing Drive. Project Design Features would prevent construction light from spilling over onto sensitive uses. Therefore, construction light spillover impacts on Area 4 would be less than significant.

Areas 5

There are no light-sensitive uses adjacent to Area 5. Therefore, no construction light spillover impacts on Area 5 would occur.

Area 6

There are no light-sensitive uses adjacent to Area 6. Therefore, no construction light spillover impacts on Area 6 would occur.

Area 7

There are no light-sensitive uses adjacent to Area 7. Therefore, no construction light spillover impacts on Area 7 would occur.

Area 8

There are no light-sensitive uses adjacent to Area 8. The Westchester Golf Course located to the north across Westchester Parkway is already substantially illuminated with flood lights and is not a light-sensitive use as it does not require minimal illumination for proper function. Therefore, no construction light spillover impacts on Area 8 would occur.

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Area 9

There are no light-sensitive uses adjacent to Area 9. The adjacent commercial parking structure located to the east along Sepulveda Westway is not a light-sensitive use as it is already substantially illuminated and does not require minimal lighting for proper function, physical comfort, or commerce. Therefore, no construction light spillover impacts on Area 9 would occur.

Area 10

Building construction is prohibited within Area 10 as the entire Area is within the Limited Development Area. Therefore, no construction light spillover impacts on Area 10 would occur.

Operation

Operation of the proposed Project would introduce new lighting on the Project site to increase visibility, promote safety, and enhance the nighttime environment. Proposed Project light sources would consist of exterior lighting along pedestrian walkways and the Paseo, vehicle driveways, and parking lots, as well as lighting for signage, security, architectural, and landscaping purposes.

Existing street lights would remain, while new street lights would be coordinated with the City of Los Angeles Bureau of Street Lighting to maintain appropriate and safe lighting on sidewalks and roadways, while minimizing light spillover on adjacent properties. Proposed Project lighting would comply with all applicable LAMC lighting standards. Outdoor lights for parking areas would be designed to reflect light away from adjacent streets or property. Exterior light would be designed to not generate a light intensity greater than two foot-candles onto specified off-site habitable or recreational uses. Further, signage illumination would be limited to a light intensity of three foot-candles above ambient lighting, as measured at the property line of the nearest residentially-zoned property.

Additionally, the proposed Project Design Features control new lighting that may be introduced with the proposed Project. The following Project Design Features apply to light spillover:

- Lighting is required to be designed to provide ambiance, safety, and security without unnecessary spillover or glare onto adjacent properties.
- Indirect wall lighting or “wall washing” and overhead down lighting may be used to help reduce light trespass into adjacent properties.
- Spotighting or glare from any site lighting is required to be shielded from adjacent properties and directed at a specific object or target area.
- Exposed bulbs are prohibited.
- When security lighting is necessary, it is required to be recessed, hooded, and located to illuminate only the intended area.
- Off-site glare and light trespass shall be prevented.
- Glare or light trespass is prohibited on any adjacent streets, or within any adjacent properties.
- Lighting mounted above ten (10) feet from -finish grade shall incorporate a full cut-off shield fixture.
- Service area lighting shall be contained within the service yard boundaries and enclosure walls.

- No light spillover shall occur outside the service area.
- Spotlighting or glare from any site lighting shall be shielded from adjacent properties and directed at a specific object or target area.

Impacts specific to each District and each Area are discussed below.

LAX Northside Center District

Area 11

Area 11 operations lighting would include new lighting for parking lots, the Paseo, pedestrian walkways, and exterior lighting for Commercial Mixed Use land uses. Light-sensitive uses adjacent to Area 11 include residences located directly to the north across La Tijera. Residences are separated from Area 11 by private walls, La Tijera, landscaping along the south side of La Tijera, and the soundwall along the northern edge of Area 11. Buildings on Area 11 are required to be set back 15 feet from Sepulveda Avenue/La Tijera. Additionally, buildings within Area 11 are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setback and away from existing light-sensitive residences to the north. The existing soundwall and landscaping north of the soundwall would remain. Operations lighting in Area 11 would be shielded from light-sensitive uses by the existing soundwall and would take place away from light-sensitive uses per the proposed Project's setback and building location requirements (**Figure 4.1-48**). The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features and would not spillover onto nearby sensitive uses. Therefore, operational light spillover impacts on Area 11 would be less than significant.

Figure 4.1-48 – Proposed Lighting Elevation and Plan for Area 11



Source: Rios Clementi Hale Studios, 2014

Area 12A East

Area 12A East operations lighting would include new lighting for parking lots, the Paseo, pedestrian walkways, and exterior lighting for Commercial Mixed Use land uses. Light-sensitive uses adjacent to Area 12A East include residences located directly to the north across 88th Street and the Westchester-Emerson Adult School located directly to the north along 88th Place. Residences are separated from Area 12A East by private walls, 88th Street, landscaping along the south side of 88th Street, and the soundwall along the northern edge of Area 12A East. Operation lighting at the LAFD Station Number 5, located directly south of the Westchester-Emerson Adult School, would not change. Buildings on Area 12A East are required to be set back 30 feet from West 88th Street and 15 feet from La Tijera/West 88th Place. Additionally,

4.1 Aesthetics

buildings within Area 12A East are required to be located adjacent to the Westchester Parkway and La Tijera setback. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features and would not spillover onto nearby sensitive uses. Therefore, operational light spillover impacts on Area 12A East would be less than significant.

Area 12A West

Area 12A West operations lighting would include new lighting for parking lots, the Paseo, pedestrian walkways, and exterior lighting for Civic and Community land uses. Area 12A West does not have adjacent light-sensitive uses as Westchester Parkway and airport support uses are located to the south, the heavily illuminated Westchester Golf Course is located to the west and north, and LAFD Station Number 5 and vacant Area 12A East are located to the east. Therefore, no operational light spillover impacts on Area 12A West would occur.

Area 12B

Area 12B operations lighting under the proposed Project would be the same as under existing conditions as lighting associated with the existing Westchester Golf Course and clubhouse would remain in its existing location and configuration. Therefore, no operational light spillover impacts would occur on Area 12B.

Area 13

Area 13 operations lighting would include new lighting for parking lots, pedestrian walkways, and exterior lighting for Civic and Community land uses. Operational lighting associated with the First Flight Child Development Center would not change under the proposed Project. Adjacent light-sensitive uses include multi-family residences to the south and west. These uses are separated from Area 13 by Lincoln Boulevard, which is an existing, well lit corridor. Additionally, buildings on Area 13 would be set back 15 feet from Lincoln Boulevard. The Westchester Golf Course to the east and First Flight Child Development Center to the north are not light-sensitive uses because neither requires minimal nighttime illumination for proper function, physical comfort, or commerce. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features and would not spillover onto nearby sensitive uses. Therefore, operational light spillover impacts on Area 13 would be less than significant.

LAX Northside Campus District

Area 1

Area 1 operations lighting would include new lighting for parking lots, the Paseo, pedestrian walkways, and exterior lighting for Office, Research and Development or Open Space land uses. Existing lighting associated with Jet Pets would remain in its existing location and configuration and would not change with the proposed Project. Adjacent light-sensitive uses include multi-family residences to the north and open space to the west. The Saint Bernard High School football field and parking lots located to the east contain flood lights and are not light-sensitive uses because they already use nighttime lighting and do not require minimal illumination for proper function, physical comfort, or commerce. A 20 foot wide Buffer is designated along the northern edge of Area 1 that would separate and screen operational activities from adjacent residences. Additionally, buildings are required to be set back 80 feet from the Buffer, for a total 100 feet of separation of buildings from existing residences. No changes to operations lighting would take place on the western edge of Area 1 adjacent to Pershing Drive and existing open space. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features and would not spillover onto nearby

sensitive uses. Therefore, operational light spillover impacts on Area 1 would be less than significant.

Area 2

Area 2 operations lighting would include new lighting for parking lots, the Paseo, pedestrian walkways, and exterior lighting for Office, Research and Development, Open Space, and Community and Civic land uses. Adjacent light-sensitive uses include single-family residences to the north and multi-family residences to the east. The Saint Bernard High School baseball field and parking lots located to the west contain flood lights and are not light-sensitive uses because they already use nighttime lighting and do not require minimal illumination for proper function, physical comfort, or commerce. The Otis College parking structure to the east is not a light-sensitive use as it also contains illumination and does not require minimal illumination for proper function. The proposed Project requires a 100-foot Buffer along the northern edge of Area 2 adjacent to residences and a 20 foot setback from the Buffer where buildings are prohibited. Additionally, buildings within Area 2 are required to be located adjacent to the Westchester Parkway, La Tijera, and Sepulveda Westway setbacks. Operational lighting may be visible from the backyards of some directly adjacent residential uses to the north. However, new lighting would be separated from residences by the Buffer and setbacks, would be shielded with new landscaping, and would be designed to avoid light spillover (**Figure 4.1-49**). The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features and would not spillover onto nearby sensitive uses. Therefore, operational light spillover impacts on Area 2 would be less than significant.

Figure 4.1-49 – Proposed Lighting Elevation and Plan for Area 2



Source: Rios Clementi Hale Studios, 2014

Area 3

Area 3 operations lighting would include new lighting for parking lots, pedestrian walkways, and exterior lighting for Office, Research and Development land uses. Adjacent light-sensitive uses include multi-family residences to the north and east. Buildings are required to be set back 20 feet from the north and east edges of the Area. Operational lighting may be visible from the balconies of some directly adjacent residential uses to the north and east. However, new lighting would be separated from residences by the setbacks, would be similar in nature to existing street lighting, and would be designed to avoid light spillover. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features and

4.1 Aesthetics

would not spillover onto nearby sensitive uses. Therefore, operational light spillover impacts on Area 3 would be less than significant.

LAX Northside Airport Support District

Area 4

Area 4 operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses. Open space located to the west across Pershing Drive is the only adjacent light-sensitive use. Buildings in Area 4 are required to be set back 50 feet from South Pershing Drive. The proposed Project's lighting sources would comply with LAMC requirements and the Project Design Features, would be designed to avoid light spillover, and would be separated from sensitive open space uses to the west by Pershing Drive and the 50 foot setback. Therefore, operational light spillover impacts on Area 4 would be less than significant.

Area 5

Area 5 operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses. There are no light-sensitive uses adjacent to Area 5. Therefore, no operational light spillover impacts on Area 5 would occur.

Area 6

Area 6 operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses. There are no light-sensitive uses adjacent to Area 6. Therefore, no operational light spillover impacts on Area 6 would occur.

Area 7

Area 7 operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses. There are no light-sensitive uses adjacent to Area 7. Therefore, no operational light spillover impacts on Area 7 would occur.

Area 8

Area 8 operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses. There are no light-sensitive uses adjacent to Area 8. The Westchester Golf Course located to the north across Westchester Parkway is already substantially illuminated with flood lights and is not a light-sensitive use as it does not require minimal illumination for proper function. Therefore, no operational light spillover impacts on Area 8 would occur.

Area 9

Area 9 operations lighting would include new lighting for parking lots and exterior lighting for Airport Support uses. Existing lighting associated with the radar facility would remain in its current location and configuration. There are no light-sensitive uses adjacent to Area 9. The adjacent commercial parking structure located to the east along Sepulveda Westway is not a light-sensitive use as it is already substantially illuminated and does not require minimal lighting for proper function, physical comfort, or commerce. Therefore, no operational light spillover impacts on Area 9 would occur.

Area 10

Building construction is prohibited within Area 10 as the entire Area is within the Limited Development Area. Therefore, operational light spillover impacts would not occur on Area 10.

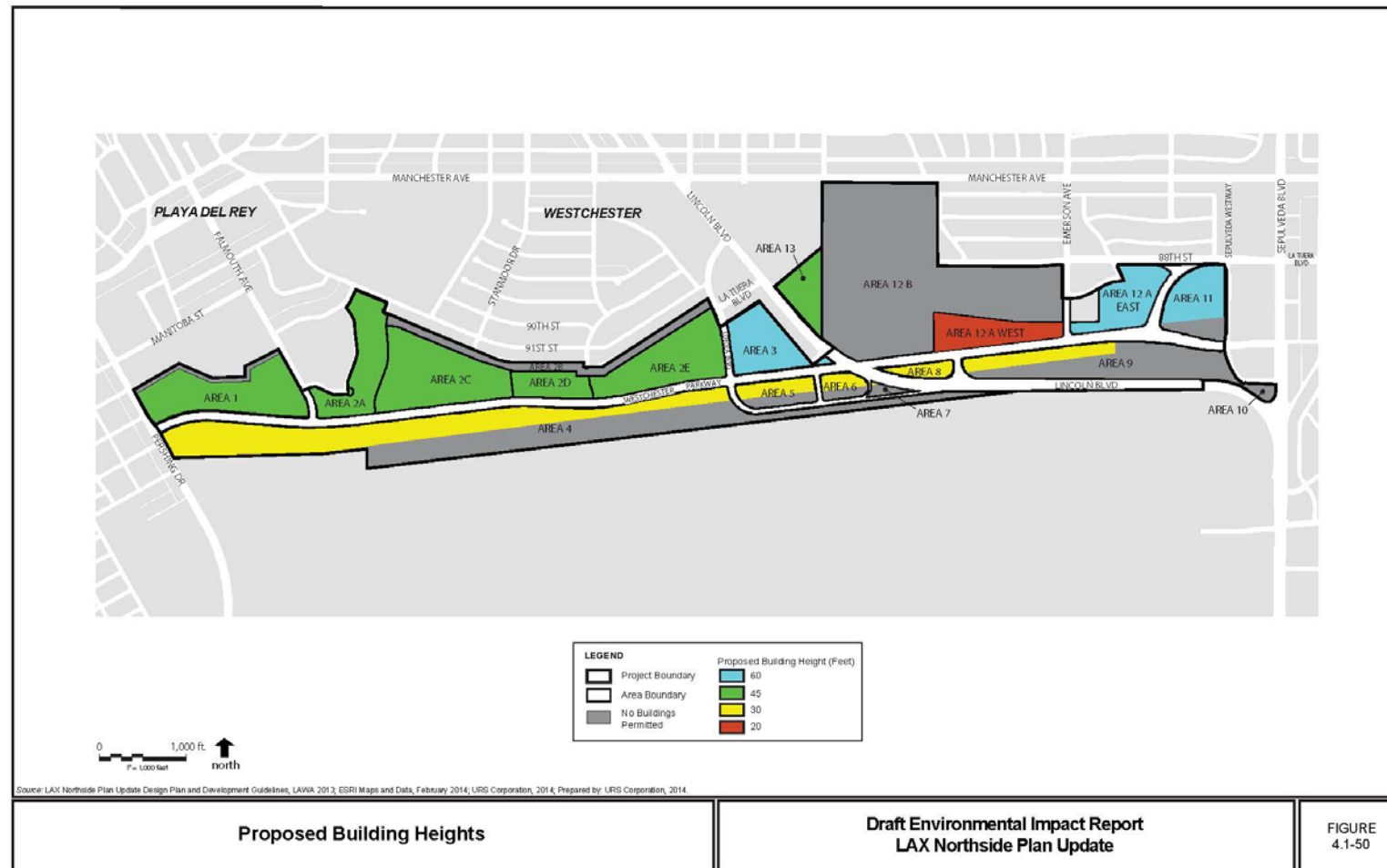
4.1.3.5.2 Shading Impacts

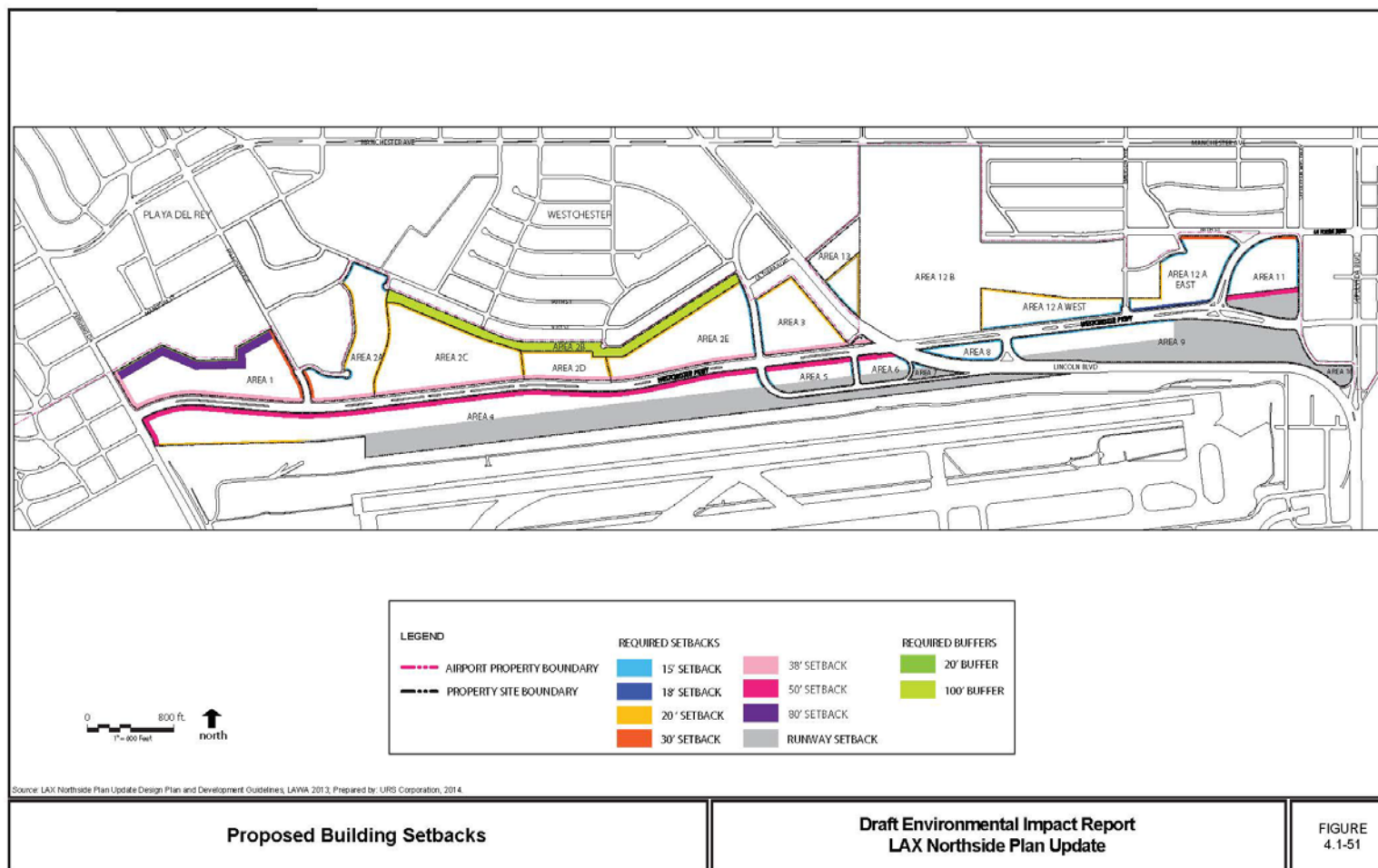
According to the 2006 City of Los Angeles CEQA Thresholds Guide, the effect of shadows must be evaluated for structures over 60 feet tall and within a distance of three times their height to shadow-sensitive land uses on the north, northwest, or northeast. The proposed LAX Northside Design Guidelines and Standards would not permit any structures over 60 feet in height. However, shading diagrams were created to evaluate the maximum potential shadow cast by the maximum allowable type of development on the Project site.

Several physical characteristics of the proposed Project are relevant to Shading Impacts. These primarily include building heights, building setbacks, and building setbacks. **Figure 4.1-50** below shows the building heights permitted in each area of the Project site. **Figure 4.1-51** below shows the building setbacks for the Project site.

The Shading Diagrams analyze the two most extreme conditions under which shadows would be produced: the summer solstice and the winter solstice. Per the City of Los Angeles CEQA Thresholds Guide, a potential impact would occur if shadow-sensitive would be shaded by proposed Project-related structures for more than three hours between the hours of 9:00 a.m. and 3:00 p.m. Pacific Standard Time (between late October and early April), or for more than four hours between the hours of 9:00 a.m. and 5:00 p.m. Pacific Daylight Time (between early April and late October). The winter solstice (December 21) is used to calculate potential impacts between late October and early April, and the summer solstice (June 21) is used to determine impacts between early April and late October. As the exact placement of structures is not known, the shade and shadow analysis includes the full developable portion of several areas that is not including setbacks, raised to the maximum height allowed. The proposed Project would not permit actual development on this scale as it would not meet the Project Design Features and would exceed permitted square footages for the proposed Project. However, use of this building envelope provides a very conservative analysis of the Project site. Any structure built by the proposed Project would cast an equal or, most likely, lesser shadow than the models predict. This approach has been taken in Area 1, 2C, 2D, 2E, 4, 5, 6, 7, 8, 9, and 10 and is referred to as the proposed maximum development of the proposed Project.

In Areas 2A, 3, 11, 12A East, 12A West, and 13, the shadow model analysis has been refined to follow the Illustrative Site Plan shown in Figure 2-6 of this EIR. Rather than showing the proposed maximum development of each area, the shadow models show an analysis for potential development that would be compliant with the permit requirements and Project Design Features of the proposed Project. The actual development of the proposed Project would potentially include different locations, footprints, and massings of structures, but would be similar in character to the Illustrative Site Plan. In addition, during specific project review, the layout of the Project site would be compared to this analysis to ensure that the specific development of the proposed Project would have shade and shadow impacts equal to or lesser than (but not greater than) the proposed development evaluated below. This approach is referred to in the analysis as the proposed reasonably foreseeable development of the proposed Project.





Construction

LAX Northside Center District

Area 11

The proposed Project in Area 11 would construct buildings up to a maximum height of 60 feet. Construction of these buildings would require erection of scaffolding and usage of heavy construction equipment, including crane equipment. Usage of scaffolds and tall construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms, including building stepbacks required for Area 11.

Existing fencing is chain link with screening. The Project site would be fenced off during construction using chain link fencing with screening, the same as existing conditions. Fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. Therefore, construction impacts related to casting shadows on sensitive uses would be less than significant.

Area 12A East

The proposed Project in Area 12A East would construct buildings up to a maximum height of 60 feet. Construction of these buildings would require erection of scaffolding and usage of heavy construction equipment, including crane equipment. Usage of scaffolds and tall construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link but is not screened. The Project site would be fenced off during construction using chain link fencing with screening. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. Therefore, construction impacts related to casting shadows on sensitive uses would be less than significant.

Area 12A West

The proposed Project in Area 12A West would construct buildings up to a maximum height of 20 feet. Construction of these buildings would require erection of scaffolding and usage of standard construction equipment. Usage of scaffolds and construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is partially screened with vegetation. The Project site would be fenced off during construction using chain link fencing with screening. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. Therefore, construction impacts related to casting shadows on sensitive uses would be less than significant.

Area 12B

The proposed Project would not involve construction in Area 12B. Therefore, construction impacts related to casting shadows on sensitive uses would not occur.

Area 13

The proposed Project in Area 13 would construct buildings up to a maximum height of 45 feet. Construction of these buildings would require erection of scaffolding and usage of heavy construction equipment, including crane equipment. Usage of scaffolds and tall construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is not screened. The Project site would be fenced off during construction using chain link fencing with screening. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. Therefore, construction impacts related to casting shadows on sensitive uses would be less than significant.

LAX Northside Campus District

Area 1

The proposed Project in Area 1 would construct buildings up to a maximum height of 45 feet. Construction would be separated from residential properties to the north by a 20-foot buffer area and 80-foot setback. Construction of these buildings would require erection of scaffolding and usage of heavy construction equipment, including crane equipment. Usage of scaffolds and tall construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is not screened. The Project site would be fenced off during construction using chain link fencing with screening. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. Therefore, construction impacts related to casting shadows on sensitive uses would be less than significant.

Area 2A

The proposed Project in Area 2A would construct buildings up to a maximum height of 45 feet. Construction of these buildings would require erection of scaffolding and usage of heavy construction equipment, including crane equipment. Usage of scaffolds and tall construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is not screened. The Project site would be fenced off during construction using chain link fencing with screening. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. Therefore, construction impacts related to casting shadows on sensitive uses would be less than significant.

4.1 Aesthetics

Area 2B

The proposed Project would not involve construction of buildings in Area 2B. Area 2B is designated as a 100-foot Buffer. Therefore, construction impacts related to casting shadows on sensitive uses would not occur.

Area 2C

The proposed Project in Area 2C would construct buildings up to a maximum height of 45 feet. Construction would be separated from residential properties to the north by the 100-foot Buffer in Area 2B. Construction of these buildings would require erection of scaffolding and usage of heavy construction equipment, including crane equipment. Usage of scaffolds and tall construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is not screened. The Project site would be fenced off during construction using chain link fencing with screening. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. Therefore, construction impacts related to casting shadows on sensitive uses would be less than significant.

Area 2D

The proposed Project in Area 2D would construct buildings up to a maximum height of 45 feet. Construction would be separated from residential properties to the north by the 100-foot Buffer in Area 2B. Construction of these buildings would require erection of scaffolding and usage of heavy construction equipment, including crane equipment. Usage of scaffolds and tall construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is not screened. The Project site would be fenced off during construction using chain link fencing with screening. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. Therefore, construction impacts related to casting shadows on sensitive uses would be less than significant.

Area 2E

The proposed Project in Area 2E would construct buildings up to a maximum height of 45 feet. Construction would be separated from residential properties to the north by the 100-foot Buffer in Area 2B. Construction of these buildings would require erection of scaffolding and usage of heavy construction equipment, including crane equipment. Usage of scaffolds and tall construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is not screened. The Project site would be fenced off during construction using chain link fencing with screening. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time.

Therefore, construction impacts related to casting shadows on sensitive uses would be less than significant.

Area 3

The proposed Project in Area 3 would construct buildings up to a maximum height of 60 feet. Construction of these buildings would require erection of scaffolding and usage of heavy construction equipment, including crane equipment. Usage of scaffolds and tall construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is not screened. The Project site would be fenced off during construction using chain link fencing with screening. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. Therefore, construction impacts related to casting shadows on sensitive uses would be less than significant.

LAX Northside Airport Support District

Area 4

The proposed Project in Area 4 would construct buildings up to a maximum height of 30 feet. Construction of these buildings would require erection of scaffolding and usage of standard construction equipment. Usage of scaffolds and construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is not screened. The Project site would be fenced off during construction using chain link fencing with screening. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. There are no shadow-sensitive uses, such as schools or residences in close proximity to Area 4 that would be affected by construction shadows. Therefore, construction impacts related to casting shadows on sensitive uses would not occur.

Area 5

The proposed Project in Area 5 would construct buildings up to a maximum height of 30 feet. Construction of these buildings would require erection of scaffolding and usage of standard construction equipment. Usage of scaffolds and construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link with screening. The Project site would be fenced off during construction using chain link fencing with screening, the same as existing conditions. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. There are no shadow-sensitive uses, such as schools or residences in close proximity to Area 5 that would be affected by construction shadows. Therefore, construction impacts related to casting shadows on sensitive uses would not occur.

4.1 Aesthetics

Area 6

The proposed Project in Area 6 would construct buildings up to a maximum height of 30 feet. Construction of these buildings would require erection of scaffolding and usage of standard construction equipment. Usage of scaffolds and construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is not screened. The Project site would be fenced off during construction using chain link fencing with screening, the same as existing conditions. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. There are no shadow-sensitive uses, such as schools or residences in close proximity to Area 6 that would be affected by construction shadows. Therefore, construction impacts related to casting shadows on sensitive uses would not occur.

Area 7

The proposed Project in Area 7 would construct buildings up to a maximum height of 30 feet. Construction of these buildings would require erection of scaffolding and usage of standard construction equipment. Usage of scaffolds and construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

There is no existing fencing. The Project site would be fenced off during construction using chain link fencing with screening, the same as existing conditions. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. There are no shadow-sensitive uses, such as schools or residences in close proximity to Area 7 that would be affected by construction shadows. Therefore, construction impacts related to casting shadows on sensitive uses would not occur.

Area 8

The proposed Project in Area 8 would construct buildings up to a maximum height of 30 feet. Construction of these buildings would require erection of scaffolding and usage of standard construction equipment. Usage of scaffolds and construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is not screened. The Project site would be fenced off during construction using chain link fencing with screening, the same as existing conditions. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. There are no shadow-sensitive uses, such as schools or residences in close proximity to Area 8 that would be affected by construction shadows. Therefore, construction impacts related to casting shadows on sensitive uses would not occur.

Area 9

The proposed Project in Area 9 would construct buildings up to a maximum height of 30 feet. Construction of these buildings would require erection of scaffolding and usage of standard construction equipment. Usage of scaffolds and construction equipment has the potential to cast shadows; however, construction equipment would be expected to be moved throughout the work day and the construction process, and scaffolding would follow building forms.

Existing fencing is chain link and is not screened. The Project site would be fenced off during construction using chain link fencing with screening, the same as existing conditions. However, fencing would follow standard construction practices and would not cast significant shadows. Construction would be temporary, and would not cast shadows on sensitive uses for a substantial amount of time. There are no shadow-sensitive uses, such as schools or residences in close proximity to Area 9 that would be affected by construction shadows. Therefore, construction impacts related to casting shadows on sensitive uses would not occur.

Area 10

The proposed Project would not involve construction of buildings in Area 10. Therefore, construction impacts related to casting shadows on sensitive uses would not occur.

Operations

LAX Northside Center District

Area 11

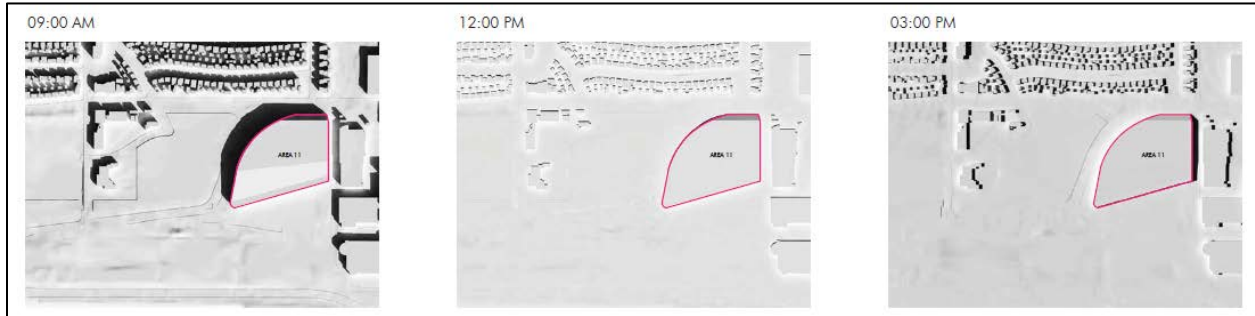
As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 11 would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 60 feet (approximately five stories);
- Buildings that would be located adjacent to the 88th Street and La Tijera property line would be stepped back by one foot for each additional foot of height above 15 feet;
- Building setbacks would be:
 - 30 feet for potential buildings developed along 88th Street and La Tijera Boulevard (northern edge of Area 11),
 - 15 feet for potential buildings developed along Sepulveda Westway and La Tijera respectively (eastern and western edges respectively), and
 - 50 foot setback and Limited Development Area for potential buildings developed along the southern portion of Area 11,
- Buildings within Area 11 would be located adjacent to the Sepulveda Westway and La Tijera setback.

Given the maximum building massing permitted by these physical restrictions, shadows potentially cast by the proposed Project during the summer would not reach any off-site shadow-sensitive uses during the hours of 9:00am to 3:00pm as shown in **Figure 4.1-52** below. The proposed maximum development analyzed includes the setback discussed above, a required Project Design Feature for the proposed Project.

4.1 Aesthetics

**Figure 4.1-52 – Summer Shadows Cast
by Proposed Maximum Development of Area 11, 9:00am to 3:00pm**



Source: Rios Clementi Hale Studios, 2014

Shadows would reach one building east of Area 11 at 6:00pm in the summer (**Figure 4.1-53**). This structure is a commercial use and is not considered a shadow-sensitive use. In addition, no shadow from the proposed Project would reach the commercial building until later than 3:00pm. The building would therefore be in shadow during the sensitive time period (9:00am to 5:00pm) for no longer than two hours.

Shadows would not reach any shadow-sensitive uses on the northern, western, eastern, or southern edges of Area 11. Therefore, impacts related to shading between early April and late October in Area 11 would not occur.

**Figure 4.1-53 – Summer Shadows Cast
by Proposed Maximum Development of Area 11, 6:00pm**



Source: Rios Clementi Hale Studios, 2014

Winter shadows would be cast to the east, north, and west of Area 11. No shadows would be cast south of Area 11 (**Figure 4.1-54**). At 9:00am shadows would cast to the northwest onto adjacent streets and Area 12A East, part of the proposed Project. Shadows would not reach the residential properties on the northern side of La Tijera. At 12:00pm, shadows would only reach adjacent streets and would not reach any shadow-sensitive uses. At 3:00pm, shadows would reach the commercial building east of Area 11 discussed above as well as adjacent streets (La Tijera and Sepulveda Westway). Residences located on the northern edge of La Tijera would remain unshaded by the proposed maximum development of Area 11. While streets would be shaded by the proposed Project, and Area 12A East and the commercial building east of Area 11 would be shaded for less than three hours during the sensitive timeframe, no shadow-

sensitive uses would be shaded by the proposed maximum development of Area 11. Therefore, impacts related to shading for 3 hours or more between the hours of 9:00am and 3:00 pm between late October and early April in Area 11 would not occur.

**Figure 4.1-54 – Winter Shadows Cast
by Proposed Maximum Development of Area 11, 9:00am to 3:00pm**



Source: Rios Clementi Hale Studios, 2014

Area 12A East

As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 12A East would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 60 feet (approximately five stories);
- Building setbacks would be:
 - 30-foot setback for potential buildings developed along 88th Street (northern edge),
 - 15-foot setback for potential buildings developed along La Tijera and West 88th Place/Liberator Avenue (eastern and northwestern edges),
 - 20-foot and 15-foot setbacks for potential buildings developed adjacent to Area 12A West (western and southwestern edges), and
 - 18-foot setback for potential buildings developed along Westchester Parkway (southern edge);
- Buildings within Area 12A East would be located adjacent to the Westchester Parkway and La Tijera setbacks.

Figure 2-6, Illustrative Site Plan, shows the implementation of these Project Design Features for Area 12A East under the proposed reasonably foreseeable development. The Project Design Features require that buildings within Area 12A West would be located adjacent to the Westchester Parkway and La Tijera setbacks. The proposed reasonably foreseeable development analyzed in the shadow analysis (Refer to Appendix M) would therefore not permit development in close proximity to Westchester Emerson Adult School.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer would not reach any off-site uses except for adjacent streets during the hours of 9:00am to 3:00pm as shown in **Figure 4.1-55** below.

4.1 Aesthetics

Figure 4.1-55 – Summer Shadows Cast by Proposed Reasonably Foreseeable Development of Area 12A East, 9:00am to 3:00pm



Source: Rios Clementi Hale Studios, 2014

Shadows would be cast beyond the adjacent streets by 6:00pm in the summer as shown in **Figure 4.1-56** below. However, shadows would only be cast on Area 11, within the Project site. Shadows would thus not be cast on any shadow-sensitive uses for four hours, or for any duration, during the summer. Therefore, impacts related to shading between early April and late October between 9:00am and 5:00pm in Area 12A East would not occur.

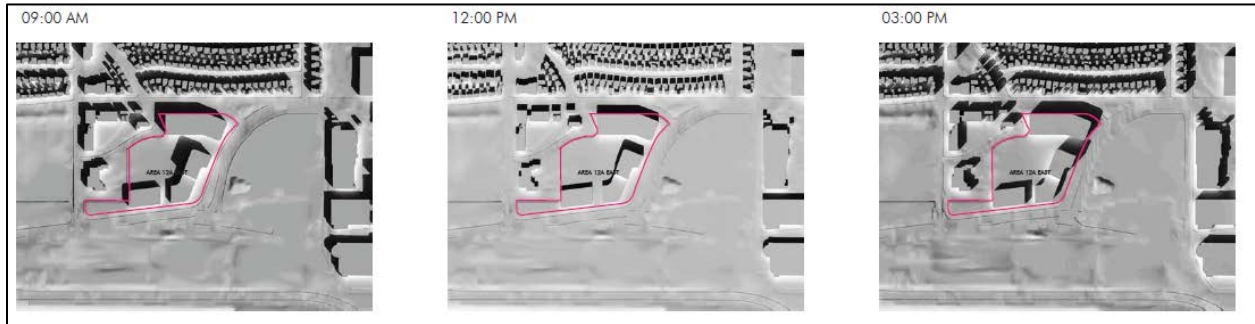
Figure 4.1-56 – Summer Shadows Cast by Proposed Reasonably Foreseeable Development of Area 12A East, 6:00pm



Source: Rios Clementi Hale Studios, 2014

Shadows cast during the winter at 9:00am would shade a portion of the Westchester Emerson Adult School, located at the northwestern corner of the intersection of West 88th Place and Liberator Avenue. At 12:00pm the Westchester Emerson Adult School would no longer be shaded, as shown in **Figure 4.1-57**.

Figure 4.1-57 – Winter Shadows Cast by Proposed Reasonably Foreseeable Development of Area 12A East, 9:00am to 3:00pm



Source: Rios Clementi Hale Studios, 2014

At 12:00pm during the winter, shadows would also reach north of the proposed reasonably foreseeable development of Area 12A East onto Westchester Parkway but would not affect any other shadow-sensitive uses. At 3:00pm, Westchester Emerson Adult School would remain unshaded shadow. Shadows would continue to be cast on Westchester Parkway, which is not a shadow-sensitive use. The shadows would not cross Westchester Parkway to shade the residential properties and backyards on the northern side of Westchester Parkway, as shown in **Figure 4.1-57**.

During the winter, the proposed reasonably foreseeable development would thus shadow Westchester Emerson Adult School. While Westchester Emerson Adult School would be in shadow at 9:00am, it would no longer be shaded by 12:00pm, and would therefore be shaded for less than three hours during the sensitive timeframe as depicted in **Figure 4.1-57**. Therefore, impacts related to shading between late October and early April between 9:00am and 3:00pm in Area 12A East would be less than significant.

Area 12A West

As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 12A West would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 20 feet (approximately two stories).
- Building setbacks would be:
 - 20-foot setback for potential buildings developed adjacent to Area 12B, Westchester Golf Course (northern and western edges); and
 - 15-foot setback for potential buildings developed along Westchester Parkway and Emerson Avenue (southern and eastern edges).
- Buildings within Area 12A West would be located adjacent to the Westchester Parkway setback.

Figure 2-6, Illustrative Site Plan, shows the implementation of these Project Design Features for Area 12A West under the proposed reasonably foreseeable development. The Project Design Features require that buildings within Area 12A West would be located adjacent to the Westchester Parkway setback. The proposed reasonably foreseeable development analyzed in the shadow analysis would therefore not encourage development in close proximity to Area 12B, the Westchester Golf Course.

4.1 Aesthetics

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer would not reach beyond Area 12A West to any shadow-sensitive uses, as shown in **Figure 4.1-58** below. Therefore, impacts related to shading between early April and late October between 9:00am and 5:00pm in Area 12A West would not occur.

Figure 4.1-58 – Summer Shadows Cast by Proposed Reasonably Foreseeable Development of Area 12A West, 9:00am to 6:00pm



Source: Rios Clementi Hale Studios, 2014

During the winter, the proposed reasonably foreseeable development of Area 12A West would cast shadows towards Area 12B (the Westchester Golf Course), a shadow-sensitive use. As shown in **Figure 4.1-59**, the shadows cast by Area 12A West would not shade Area 12B at 9:00am or at 12:00pm. Shadows would be cast onto Area 12B at 3:00pm. However, as shadows would not be cast on Area 12B until after 12pm, the shadows cast Area 12A West would not shade the shadow-sensitive Westchester Golf Course for three or more hours during the sensitive time frame (9:00am to 3:00pm). Therefore, impacts related to shading for 3 hours or more between the hours of 9:00am and 3:00 pm between late October and early April in Area 12A West would be less than significant.

Figure 4.1-59 – Winter Shadows Cast by Proposed Reasonably Foreseeable Development of Area 12A West, 9:00am to 3:00pm



Source: Rios Clementi Hale Studios, 2014

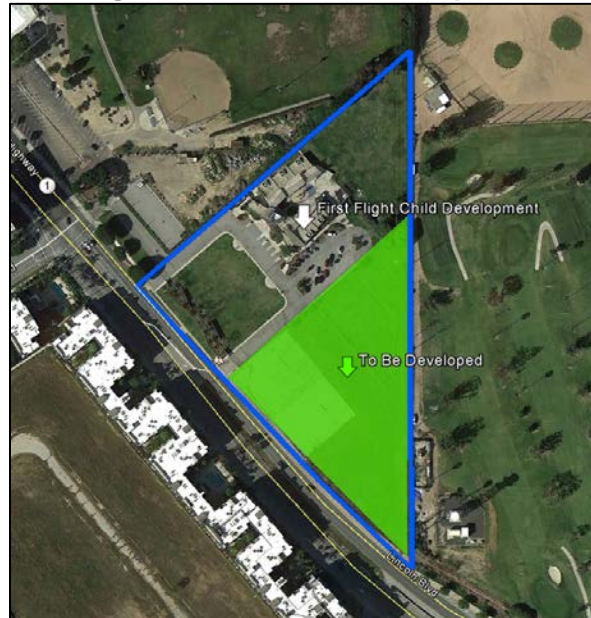
Area 12B

Area 12B is the Westchester Golf Course. The proposed Project would not change the existing use, heights, or setbacks of Area 12B. Therefore, impacts related to shading for four hours or more between the hours of 9:00am and 5:00pm between early April and late October or for three hours or more between the hours of 9:00am and 3:00 pm between late October and early April in Area 12B would not occur.

Area 13

Area 13 includes the existing First Flight Child Development use. Area 13 is triangular in shape, and First Flight Child Development is in the northern portion of Area 13, as shown in **Figure 4.1-60**. Only the southern portion, highlighted in **Figure 4.1-60**, would be developed by the proposed Project. Therefore, for Area 13, the proposed reasonably foreseeable development used in the shade analysis does not cover the full Area 13, but is only the portion that would be developed under the proposed Project.

Figure 4.1-60 – Existing Conditions and Developable Area within Area 13



Source: Google Earth, 2014

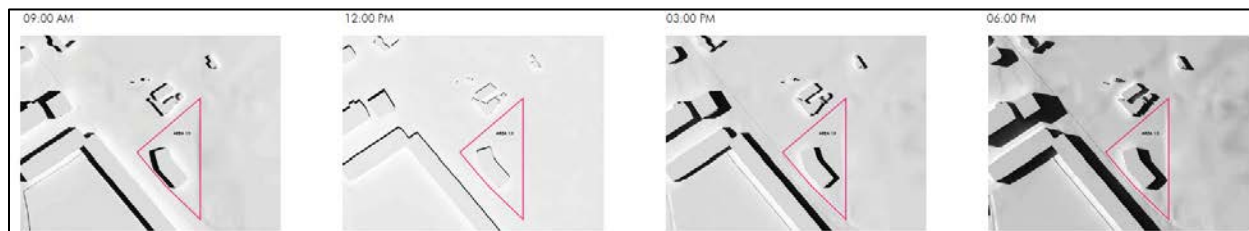
As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 13 would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 45 feet (approximately four stories).
- Building setbacks would be:
 - 20-foot setback for potential buildings developed in the southern portion of Area 13 adjacent to First Flight Child Development and Area 12B (Westchester Golf Course), and
 - 15-foot setback for potential buildings developed along Pacific Coast Highway.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer throughout the day would be limited only to the area within Area 13, and would not reach First Flight Child Development, Westchester Golf Course in Area 12B, or any other shadow-sensitive uses. **Figure 4.1-61** below shows these shadows. Therefore, impacts related to shading for four hours or more between the hours of 9:00am and 5:00 pm between early April and late October in Area 13 would not occur.

4.1 Aesthetics

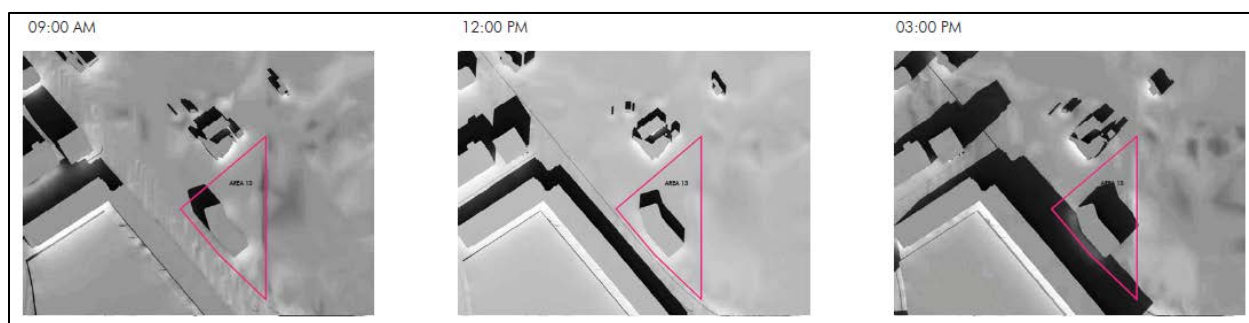
**Figure 4.1-61 – Summer Shadows Cast
by Proposed Reasonably Foreseeable Development of Area 13, 9:00am to 6:00pm**



Source: Rios Clementi Hale Studios, 2014

During the winter, the proposed reasonably foreseeable development of Area 13 would cast a shadow on First Flight Child Development at 9:00am. However, at 12:00pm, the proposed reasonably foreseeable development of Area 13 would no longer shade First Flight Child Development and would also not shade the Westchester Golf Course in Area 12B, as shown in **Figure 4.1-62** below. The Westchester Golf Course, would be minimally shadowed by 3:00pm, but First Flight Development would remain unshaded.

**Figure 4.1-62 – Winter Shadows Cast
by Proposed Reasonably Foreseeable Development of Area 13**



Source: Rios Clementi Hale Studios, 2014

As First Flight Child Development would no longer be shadowed by 12:00pm, and as the Westchester Golf Course would only be in shadow later than 12:00pm, both shadow-sensitive uses would be shaded for less than three hours during the sensitive time frame. Therefore, impacts related to shading for three hours or more between the hours of 9:00am and 3:00 pm between late October and early April in Area 13 would be less than significant.

LAX Northside Campus District

Area 1

As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 1 would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 45 feet (approximately four stories).
- Required buffers would be 20 feet from the edge of Area 1 on the northern edge (adjacent to residential properties).
- Building setbacks would be:

- 80-foot setback for potential buildings developed adjacent to residential properties north of Area 1 (northern edge);
- 30-foot setback for potential buildings developed along Falmouth Avenue (eastern edge); and
- 38-foot setback for potential buildings developed along Westchester Parkway and Pershing Drive (southern and western edges).

The required 20-foot Buffer and 80-foot setback on the northern edge of Area 1, where the Project site abuts residential properties, provides a combined 100-foot Buffer between proposed maximum development of Area 1 and any residential properties that would be shadow-sensitive uses.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer at 9:00am would only shade the setback and buffer on the northern edge of Area 1 and Pershing Drive. At 12:00pm, no off-site uses would be shaded. At 3:00pm, the proposed Project would only shade Falmouth Avenue. By 6:00pm, after the end of the sensitive time frame (9:00am to 5:00pm), the proposed maximum development of Area 1 would shade Westchester Parkway and Falmouth Avenue, but no other uses. **Figure 4.1-63** below shows the shading for these times.

**Figure 4.1-63 – Summer Shadows Cast by
Proposed Maximum Development of Area 1, 9:00am to 6:00pm**



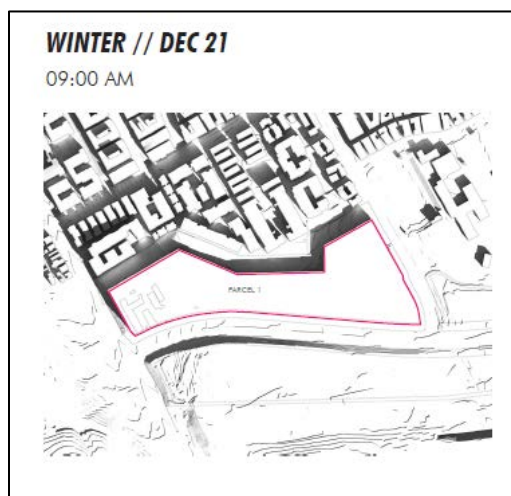
Source: Rios Clementi Hale Studios, 2014

The residential properties directly north of Area 1 would not be shaded at any time by the proposed maximum development of Area 1, and no other shadow-sensitive uses would be shaded at any time. Therefore, impacts related to shading between early April and late October between 9:00am and 5:00pm would not occur.

During the winter, the proposed maximum development of Area 1 would shade some of the residential properties adjacent to the north of Area 1. These shadows are shown in **Figure 4.1-64**.

4.1 Aesthetics

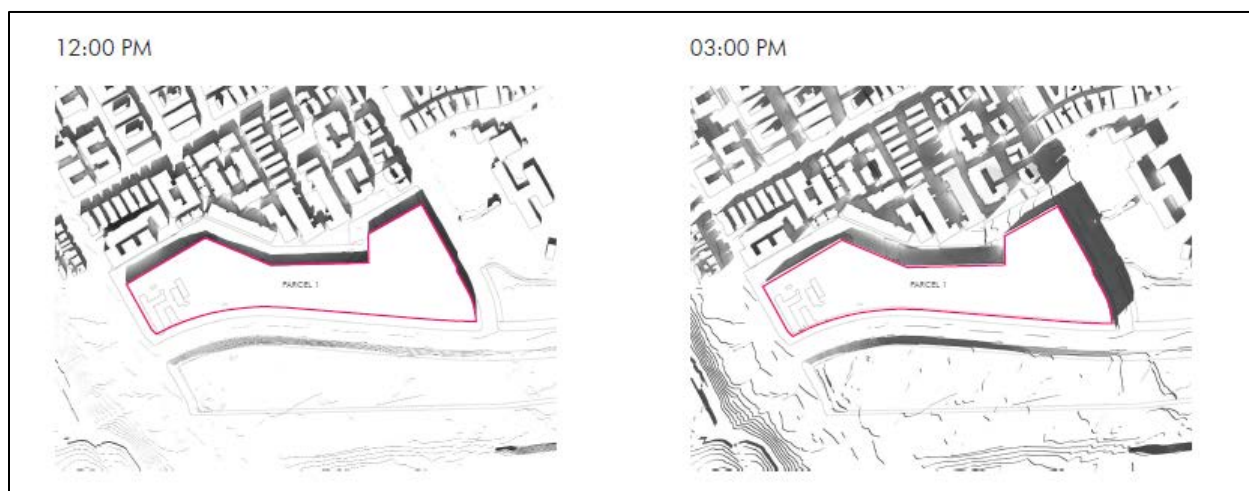
**Figure 4.1-64 – Winter Shadows Cast
by Proposed Maximum Development of Area 1, 9:00am**



Source: Rios Clementi Hale Studios, 2014

At 12:00pm, none of the residential properties would remain in shadow, and thus no shadow-sensitive uses would be shaded by the proposed maximum development of Area 1. At 3:00pm, the proposed Project would cast longer shadows, but because of the 100-foot Buffer provided by the combined setback and buffer requirements, the residential properties north of Area 1 would not be shaded. **Figure 4.1-65** below shows the shadows that occur at 12:00pm and 3:00pm.

**Figure 4.1-65 – Winter Shadows Cast
by Proposed Maximum Development of Area 1, 12:00pm to 3:00pm**



Source: Rios Clementi Hale Studios, 2014

Although residential properties would be shaded at 9:00am during the winter by the proposed maximum development of Area 1, they would no longer be in shadow by 12:00pm, and would not be shaded later in the day. The shadows on these shadow-sensitive uses would therefore be present for less than three hours during the sensitive time frame. Therefore, impacts related

to shading between late October and early April between 9:00am and 3:00pm would be less than significant.

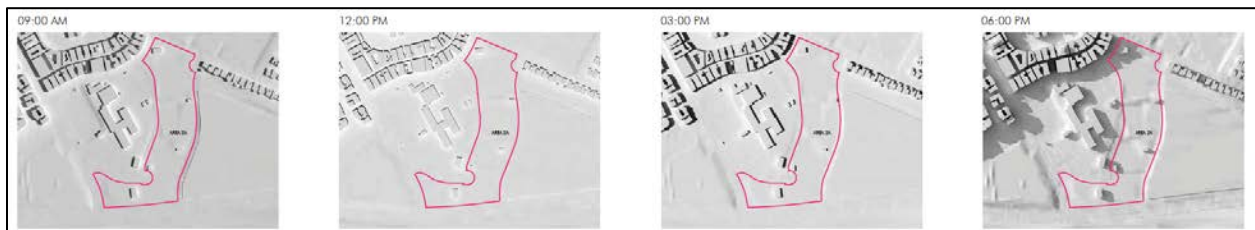
Area 2A

As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 2A would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 45 feet (approximately four stories).
- Building setbacks would be:
 - 15-foot setback for potential buildings developed along St. Bernard Street, West 91st Street, and Hastings Avenue (northern edge) and West 92nd Street (northern edge of the southern portion of Area 2A);
 - 20-foot setback for potential buildings developed along Cum Laude Avenue and adjacent to Area 2C (western and eastern edges);
 - 30-foot setback for potential buildings developed along Falmouth Avenue (western edge); and
 - 38-foot setback for potential buildings developed along Westchester Parkway (southern edge).
- Buildings within Area 2A would be located adjacent to the Westchester Parkway setback.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer would cast shadows only on Area 2A and would not shade any shadow-sensitive uses. These shadows are depicted in **Figure 4.1-66**. Therefore, impacts related to shading for more than four hours between 9:00am and 5:00pm from early April to late October would not occur.

**Figure 4.1-66 – Summer Shadows Cast
by Proposed Reasonably Foreseeable Development of Area 2A, 9:00am to 6:00pm**

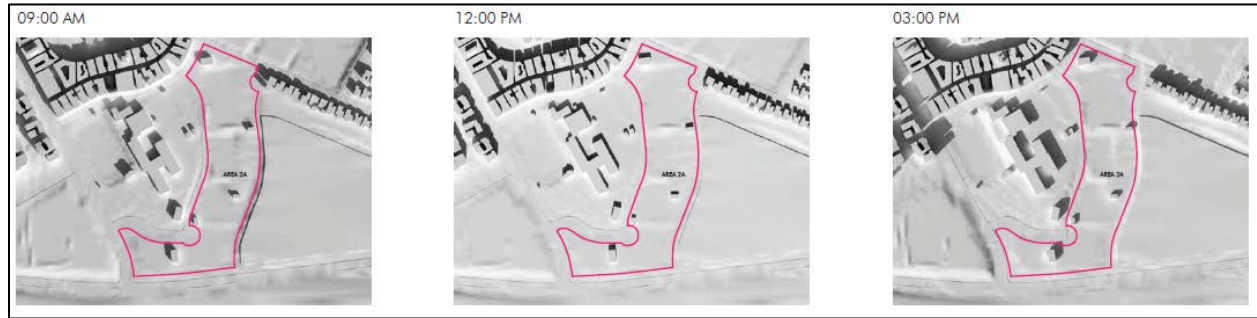


Source: Rios Clementi Hale Studios, 2014

During the winter, shadows cast by the proposed Project at 9:00am would shade Area 2A and a small portion of Saint Bernard Avenue. At 12:00pm, all shadows would remain on the Project site, within Area 2A. At 3:00pm, shadows would be cast within Area 2A as well as on a small portion of West 92nd Street. These shadows are depicted in **Figure 4.1-67**.

4.1 Aesthetics

**Figure 4.1-67 – Winter Shadows Cast
by Proposed Reasonably Foreseeable Development of Area 2A, 9:00am to 3:00pm**



Source: Rios Clementi Hale Studios, 2014

Although Saint Bernard High School is across the street from Area 2A, the proposed reasonably foreseeable development of the proposed Project would not shade this or any other shadow-sensitive use. Neither Saint Bernard Avenue nor West 92nd Street is shadow-sensitive use. Therefore, impacts related to shading between 9:00am and 3:00pm for more than three hours from late October to early April would not occur.

Area 2B

As described in the Project Design Features and shown in **Figure 4.1-51** above, Area 2B would be designated as a Buffer land use. The entirety of Area 2B would be required to remain a 100-foot Buffer. As a result, there would no new development of buildings in Area 2B. A 10-foot fence would be installed to secure the perimeter of Area 2B. However, as shown in the proposed LAX Northside Design Guidelines and Standards, proposed fencing for landscape buffer areas, including Area 2B, would not be solid and would thus not cast a substantial shadow. Therefore, impacts related to shading for four hours or more between the hours of 9:00am and 5:00pm between early April and late October or for three hours or more between the hours of 9:00am and 3:00 pm between late October and early April in Area 2B would not occur.

Area 2C

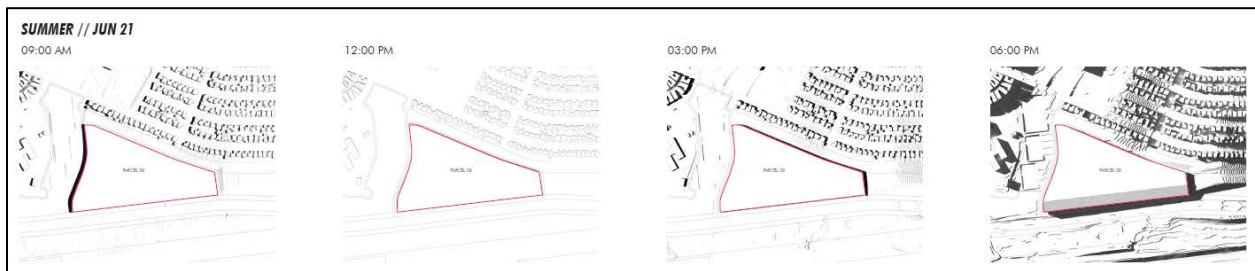
As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 2C would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 45 feet (approximately four stories).
- No buffers are established within Area 2C; however, the 100-foot Buffer zone in Area 2B is adjacent to the northern edge of Area 2C.
- Building setbacks would be:
 - 20-foot setback for potential buildings developed adjacent to Area 2A (western edge), Area 2B (northern edge), and Area 2D (eastern edge); and
 - 38-foot setback for potential buildings developed adjacent to Westchester Parkway (southern edge).
- Buildings within Area 2C would be located adjacent to the Westchester Parkway setback.

- Buildings within Area 2C shall be located with a minimum of 65 percent of the proposed Project ground floor building square footage located within 250 feet of the Westchester Parkway property line.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer at 9:00am would shade only a minimal portion of Area 2A, part of the Project site. At 12:00pm, the proposed maximum development of Area 2C would not shadow any neighboring properties or uses. At 3:00pm, minimal shadows would be cast on Area 2B (the landscape buffer) and Area 2D, part of the proposed Project. Shading of Westchester Parkway, as well as additional shading of Area 2D would occur later than 3:00pm. Thus, the proposed maximum development of Area 2C would not at any time in the sensitive time frame (9:00am to 5:00pm) cast a shadow on shadow-sensitive uses, as shown in **Figure 4.1-68** below. Therefore, impacts related to shading for more than four hours between 9:00am and 5:00pm from early April to late October would not occur.

Figure 4.1-68 – Summer Shadows Cast by Proposed Maximum Development of Area 2C



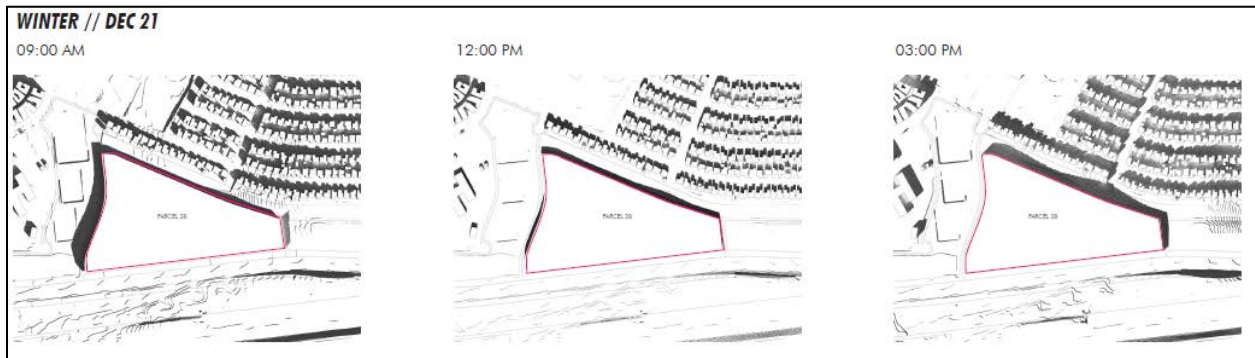
Source: Rios Clementi Hale Studios, 2014

During the winter, the proposed maximum development of Area 2C would cast shadows at 9:00am on Area 2A and Area 2B, part of the Project site. No shadow-sensitive uses would be in shadow. Area 2B would remain in shadow at 12:00pm, but is designed to be a buffer for the proposed Project and is not a shadow-sensitive use. At 3:00pm, the shadows cast by the proposed maximum development of Area 2C would reach the backyards of two residential properties adjacent to Area 2B at the southwest corner of 91st Street and Stanmoor Drive. Area 2B would remain in shadow but is not a shadow-sensitive use. As the shadow-sensitive use of the residential properties at 91st Street and Stanmoor Drive would be shaded only after 12:00pm, they would be in shadow for less than three hours during the sensitive time frame (9:00am to 3:00pm). **Figure 4.1-69** below shows the shadows cast during the sensitive time frame.

Furthermore, buildings within Area 2C would be located adjacent to the Westchester Parkway setback and would be located with a minimum of 65 percent of the proposed Project ground floor building square footage located within 250 feet of the Westchester Parkway property line. These Project Design Features would result in buildings that are not constructed as far north as the proposed maximum development, and would thus not shade the two residential shadow-sensitive uses. Therefore, impacts related to shading for three or more hours between 9:00am and 3:00pm from late October to early April would not occur.

4.1 Aesthetics

Figure 4.1-69 – Winter Shadows Cast by Proposed Maximum Development of Area 2C



Source: Rios Clementi Hale Studios, 2014

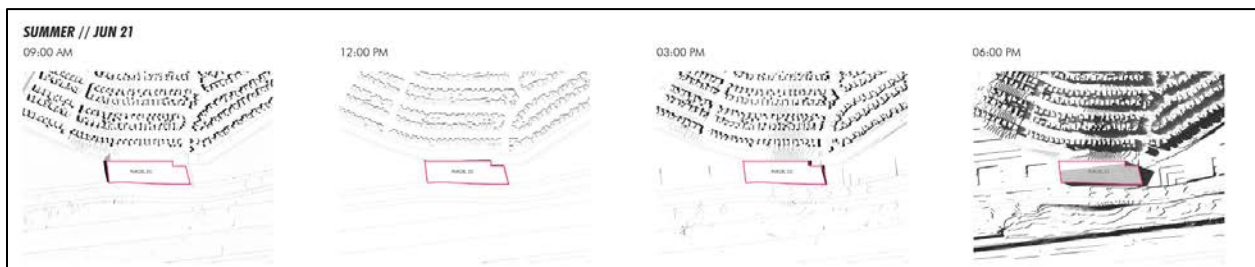
Area 2D

As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 2D would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 45 feet (approximately four stories).
- No buffers are established within Area 2D; however, the 100-foot Buffer zone in Area 2B is adjacent to the northern edge of Area 2D.
- Building setbacks would be:
 - 20-foot setback for potential buildings developed adjacent to Area 2C (western edge), Area 2B (northern edge), and Area 2E (eastern edge); and
 - 38-foot setback for potential buildings developed adjacent to Westchester Parkway (southern edge).
- Buildings within Area 2D would be located adjacent to the Westchester Parkway setback.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer at 9:00am would cast a minimal shadow on Area 2C, part of the Project site. At 12:00pm, no uses outside Area 2C would be in shadow. By 3:00pm, small portions of Area 2E, part of the Project site, would be shaded. After 5:00pm, more of Area 2E would be shaded. At no time during the day would the residential properties adjacent to the northern edge of Area 2B be shaded by the proposed maximum development of Area 2D. **Figure 4.1-70** below shows the shadows cast during the summer by the proposed maximum development of Area 2D. Therefore, impacts related to shading for more than four hours between 9:00am and 5:00pm from early April to late October in Area 2D would not occur.

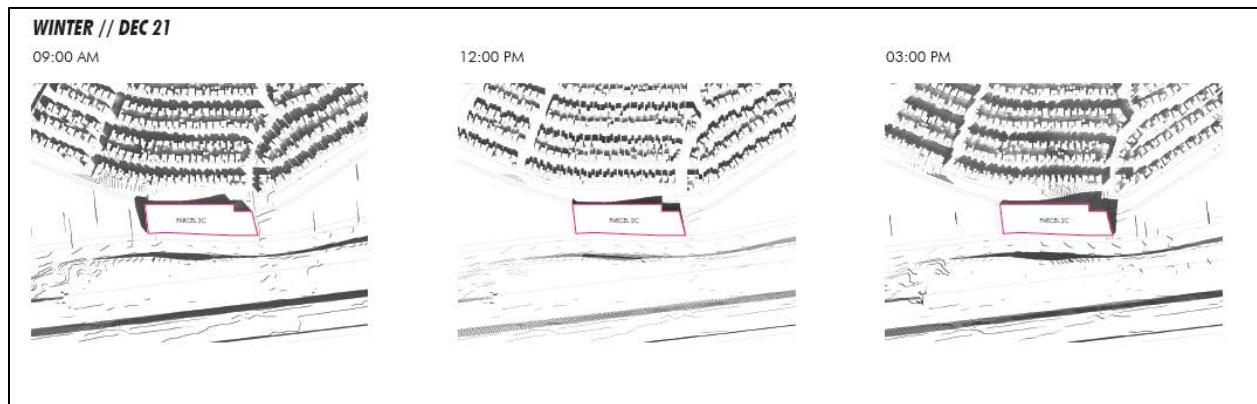
Figure 4.1-70 – Summer Shadows Cast by Proposed Maximum Development of Area 2D



Source: Rios Clementi Hale Studios, 2014

During the winter, shadows would be cast on Area 2B and Area 2C at 9:00am. These areas are part of the Project site and are not shadow-sensitive uses. At 12:00pm, Area 2B would remain in shade but Area 2C would not be shaded. At 3:00pm, Area 2B would remain in increasing shadow. However, at no time during the sensitive time frame (9:00am to 3:00pm) would the residential properties adjacent to the northern edge of Area 2B be shaded by the proposed maximum development of Area 2D. **Figure 4.1-71** below shows the shadows cast during the summer by the proposed maximum development of Area 2D. Therefore, impacts related to shading for more than three hours between 9:00am and 5:00pm from late October to early April would not occur.

Figure 4.1-71 – Winter Shadows Cast by Proposed Maximum Development of Area 2D



Source: Rios Clementi Hale Studios, 2014

Area 2E

As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 2E would have the following characteristics that would affect how much shading is generated by the proposed development:

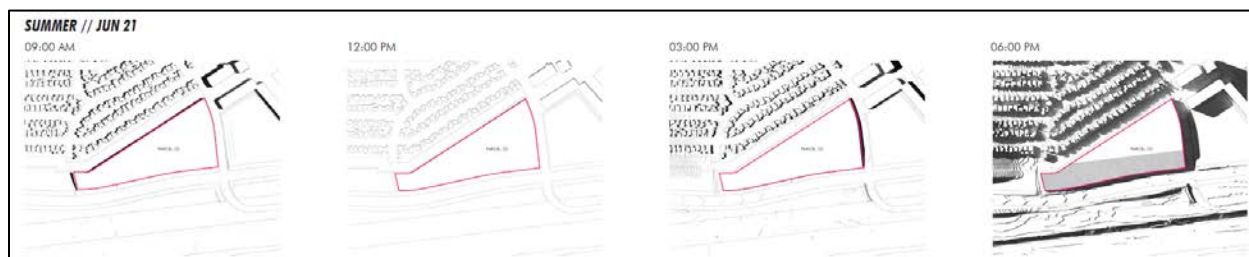
- Maximum height for any development would be 45 feet (approximately four stories).
- No buffers are established within Area 2E; however, the 100-foot Buffer zone in Area 2B is adjacent to the northern edge of Area 2E.
- Building setbacks would be:
 - 15-foot setback for potential buildings developed along Loyola Boulevard (eastern edge);
 - 20-foot setback for potential buildings developed adjacent to Area 2D (western edge) and Area 2B (northern edge); and
 - 38-foot setback for potential buildings developed along Westchester Parkway (southern edge).
- Buildings within Area 2E would be located adjacent to the Westchester Parkway setback.
- Buildings within Area 2E would be located with minimum of 65 percent of the proposed Project ground Floor building square footage located within 250 feet of the Westchester Parkway property line.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer at 9:00am would cast minimal shadows on Area 2D

4.1 Aesthetics

and Area 2B, portions of the Project site. By 12:00pm, shadows would not be cast off-site of Area 2E. At 3:00pm, minimal shading would be cast on Loyola Boulevard. At 6:00pm, after the end of the sensitive time frame, Westchester Parkway and Loyola Boulevard would be shaded, as would be a portion of Area 3, part of the Project site. **Figure 4.1-72** below shows the shading caused by the proposed maximum development of Area 2E during the summer. None of the uses that would be shadowed during the sensitive time frame (9:00am to 5:00pm) are shadow-sensitive uses. Therefore, impacts related to shading for more than four hours between 9:00am and 5:00pm from early April to late October in Area 2E would not occur.

Figure 4.1-72 – Summer Shadows Cast by Proposed Maximum Development of Area 2E

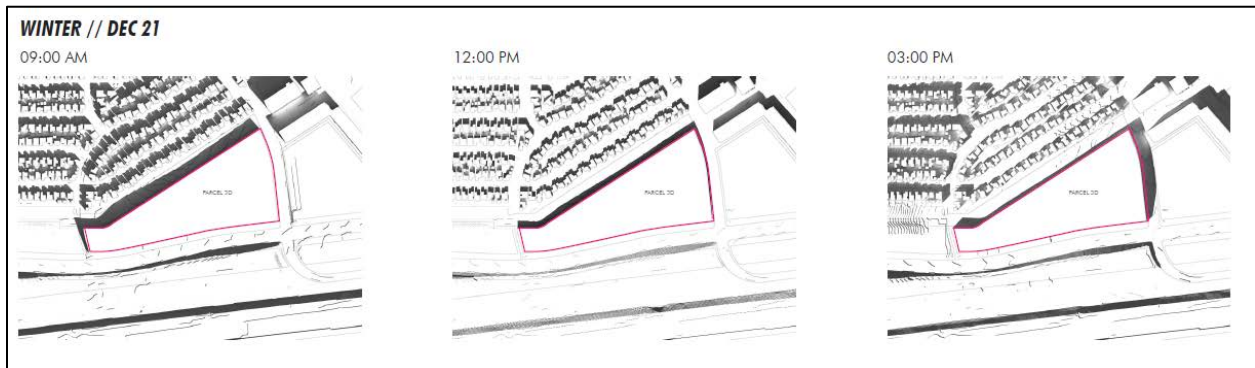


Source: Rios Clementi Hale Studios, 2014

During the winter, at 9:00am Area 2B would be shaded by the proposed maximum development of Area 2E. According to the conservative shadow model, which depicts shadows cast by elevating all of Area 2E to the 45-foot height maximum, the backyards of several residential properties adjacent to the northern edge of Area 2B would be shaded. However, the proposed LAX Northside Design Guidelines and Standards require that buildings within Area 2, including Area 2E, would be located adjacent to the Westchester Parkway setback at the southern edge of Area 2E. Buildings in Area 2E are also required to locate a minimum of 65 percent of the proposed Project ground floor building square footage within 250 feet of the Westchester Parkway property line. These restrictions on the locations and massing of proposed buildings in Area 2E would result in a proposed maximum development that would shade Area 2B, but not residential properties north of Area 2B.

At 12:00pm, Area 2B would remain in shadow, and the residential properties adjacent to the northern edge of Area 2B would remain unshaded. At 3:00pm, Area 2B, Loyola Boulevard, and Area 3 would be shaded by the proposed maximum development of Area 2E. Residential properties would not be shaded at 3:00pm. **Figure 4.1-73** below shows the shadows occurring during the winter for Area 2E. None of the uses that would be shaded by the proposed maximum development of Area 2E are shadow-sensitive uses. Therefore, impacts related to shading for more than three hours between 9:00am and 3:00pm from late October to early April would not occur.

Figure 4.1-73 – Winter Shadows Cast by Proposed Maximum Development of Area 2E



Source: Rios Clementi Hale Studios, 2014

Area 3

As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 3 would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 60 feet (approximately five stories);
- Grading in Area 3 would decrease elevations for building frontages relative to existing residential development and would provide a more accessible relationship with Westchester Parkway.
- Building setbacks would be:
 - 15-foot setback for potential buildings developed along Loyola Boulevard (western edge);
 - 20-foot setback for potential buildings developed adjacent to the residential properties north of Area 3 on La Tijera Boulevard and Lincoln Boulevard (northeastern and northwestern edges); and
 - 38-foot setback for potential buildings developed along Westchester Parkway (southern edge).

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed reasonably foreseeable development of the proposed Project during the summer at 9:00am would shade only the area within Area 3. This would continue at 12:00pm and at 3:00pm (**Figure 4.1-74**).

4.1 Aesthetics

**Figure 4.1-74 – Summer Shadows Cast
by Proposed Reasonably Foreseeable Development of Area 3, 9:00am to 3:00pm**



Source: Rios Clementi Hale Studios, 2014

**Figure 4.1-75 – Summer Shadows Cast
by Proposed Reasonably Foreseeable Development of Area 3, 6:00pm**



Source: Rios Clementi Hale Studios, 2014

The residential properties and courtyards lining the southwestern side of Lincoln Boulevard would not be shaded at 3:00pm but would be shaded at 6:00pm, and would thus potentially be in shadow during the sensitive time frame. However, as the sensitive timeframe is 9:00am to 5:00pm, the residential properties would be shaded for less than two hours (from after 3:00pm until 5:00pm). In addition, Area 3 requires a 20-foot setback from the residential properties and courtyards lining the southwestern side of Lincoln Boulevard. Implementation of this Project Design Feature would further reduce and potentially eliminate shade cast on the residential properties and courtyards. While these residential properties and courtyards may receive a limited amount of shading during the sensitive timeframe (9:00am to 5:00pm), they would be shaded for less than two hours. Therefore, impacts related to shading for more than four hours between 9:00am and 5:00pm from early April to late October in Area 3 would be less than significant.

During the winter, at 9:00am, the proposed reasonably foreseeable development of Area 3 would cast a shadow on Loyola Boulevard and the residential properties and courtyards along the southern side of La Tijera Boulevard between Loyola Boulevard and Lincoln Boulevard.

Loyola Boulevard is not a shadow-sensitive use but the residential properties are shadow-sensitive uses. At 12:00pm, these residences would remain in shade, and shadows would be cast only within Area 3. The residential properties and courtyards lining the southwestern side of Lincoln Boulevard between La Tijera Boulevard and West 94th Street would also be shaded by the proposed maximum development of Area 3 at 12:00pm (**Figure 4.1-76**).

**Figure 4.1-76 – Winter Shadows Cast by
Proposed Reasonably Foreseeable Development of Area 3, 9:00am to 3:00pm**



Source: Rios Clementi Hale Studios, 2014

The residential properties and courtyards along the southern side of La Tijera Boulevard between Loyola Boulevard and Lincoln Boulevard and the residential properties and courtyards lining the southwestern side of Lincoln Boulevard between La Tijera Boulevard and West 94th Street would remain unshaded at 3:00pm, as shown in **Figure 4.1-76**. The residential properties and courtyards lining the southwestern side of Lincoln Boulevard between La Tijera Boulevard and West 94th Street would be shaded at 3:00pm.

The proposed reasonably foreseeable development of Area 3 would result in these shadow-sensitive uses, as a whole, being potentially shaded for more than three hours during the sensitive time frame (9:00am to 3:00pm). However, each use would be shadowed for less than three hours during the full timeframe. The properties shaded at 9:00am would not be shaded at 3:00pm, and the properties shaded at 3:00pm would not be shaded at 9:00am. No residential properties or courtyards would be shaded for three hours or longer during the sensitive timeframe.

Grading in Area 3 would also decrease elevations for building frontages relative to the existing residential development. This grading strategy would reduce the duration of shadows cast on these residential properties. Therefore, impacts related to shading for more than three hours between 9:00am and 3:00pm from late October to early April would be less than significant.

LAX Northside Airport Support District

Area 4

As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 4 would have the following characteristics that would affect how much shading is generated by the proposed development:

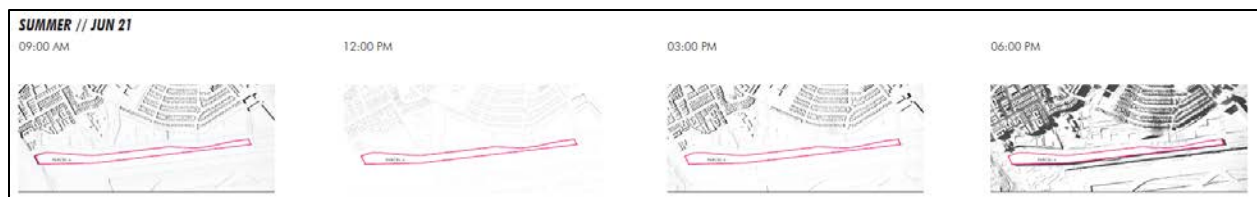
- Maximum height for any development would be 30 feet (approximately three stories).
- Building setbacks would be:
 - 15-foot setback for potential buildings developed along Northside Parkway (eastern edge of Area 4);

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- 20-foot setback for potential buildings developed adjacent to the airfield in the western portion of Area 4 (western portion of the southern edge);
 - 50-foot setback for potential buildings developed along Westchester Parkway (western edge) and Pershing Drive (western edge); and
 - Limited Development Area for the southern portion of Area 4 adjacent to the LAX North Airfield (approximately the southern half of Area 4).
- Buildings are prohibited within the Limited Development Area.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer would be negligible for all times of day. **Figure 4.1-77** shows shadows cast during the summer by the proposed maximum development of Area 4.

Figure 4.1-77 – Summer Shadows Cast by Proposed Maximum Development of Area 4

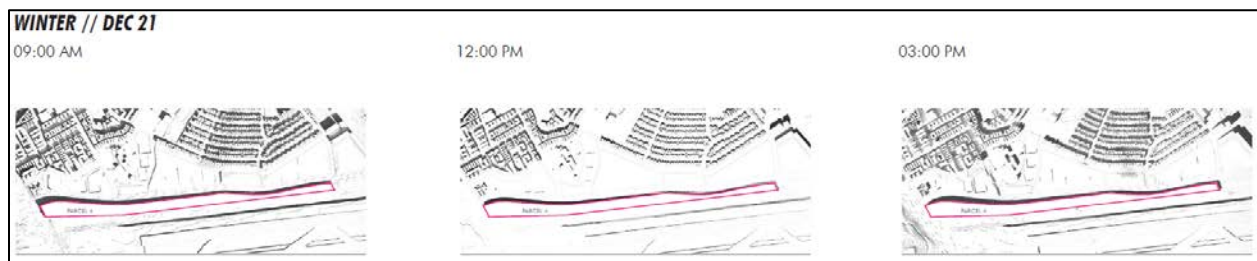


Source: Rios Clementi Hale Studios, 2014

Area 4 is surrounded by roadways on its western, northern, and eastern edges, and LAX airfield operations on its southern edge. There are no shadow-sensitive uses in proximity to Area 4. The proposed maximum development would thus not shade any shadow-sensitive uses. Therefore, impacts related to shading for more than four hours between 9:00am and 5:00pm from early April to late October in Area 4 would not occur.

During the winter, the proposed maximum development of Area 4 would cast shadows only on surrounding roadways: Pershing Drive, Westchester Parkway, and Northside Parkway. No shadow-sensitive uses would be shaded at any time, as shown in **Figure 4.1-78** below.

Figure 4.1-78 – Winter Shadows Cast by Proposed Maximum Development of Area 4



Source: Rios Clementi Hale Studios, 2014

Therefore, impacts related to shading for more than three hours between 9:00am and 3:00pm from late October to early April in Area 4 would not occur.

Area 5

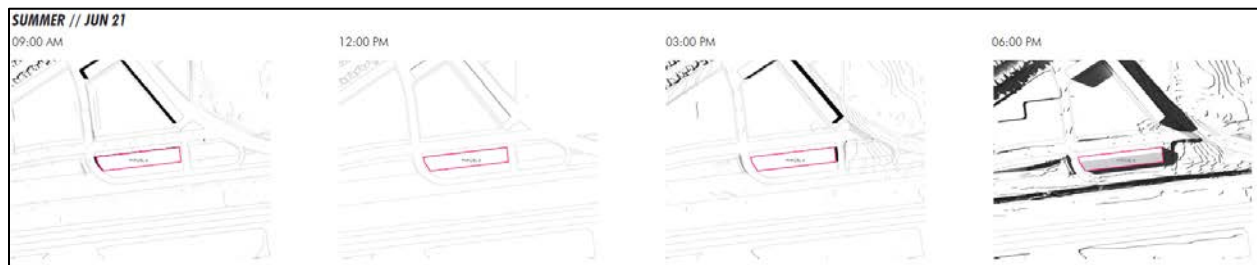
As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 5 would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 30 feet (approximately three stories).

- Building setbacks would be:
 - 15-foot setback for potential buildings developed along Northside Parkway (western edge) and Georgetown Avenue (eastern edge);
 - 50-foot setback for potential buildings developed along Westchester Parkway (northern edge); and
 - Limited Development Area for the southern portion of Area 5 along Northside Parkway (southern edge).
- Buildings are prohibited within the Limited Development Area.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer would be negligible for all times of day. **Figure 4.1-79** shows shadows cast during the summer by the proposed maximum development of Area 5.

Figure 4.1-79 – Summer Shadows Cast by Proposed Maximum Development of Area 5

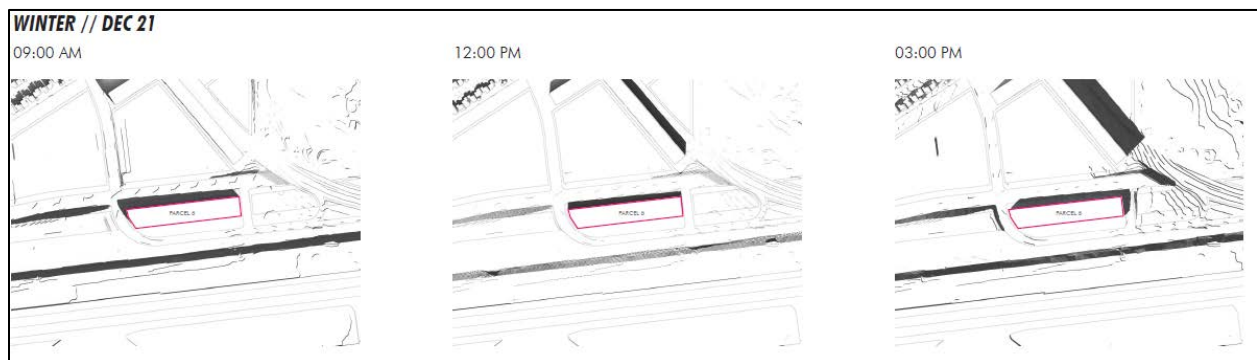


Source: Rios Clementi Hale Studios, 2014

Area 5 is surrounded by roadways on its western, northern, and eastern edges, and LAX airfield operations on its southern edge. There are no shadow-sensitive uses in proximity to Area 5. The proposed maximum development would thus not shade any shadow-sensitive uses. Therefore, impacts related to shading for more than four hours between 9:00am and 5:00pm from early April to late October in Area 4 would not occur.

During the winter, the proposed maximum development of Area 5 would cast shadows only on surrounding roadways: Pershing Drive, Westchester Parkway, and Northside Parkway. No shadow-sensitive uses would be shaded at any time, as shown in **Figure 4.1-80** below.

Figure 4.1-80 – Winter Shadows Cast by Proposed Maximum Development of Area 5



Source: Rios Clementi Hale Studios, 2014

Therefore, impacts related to shading for more than three hours between 9:00am and 3:00pm from late October to early April in Area 5 would not occur.

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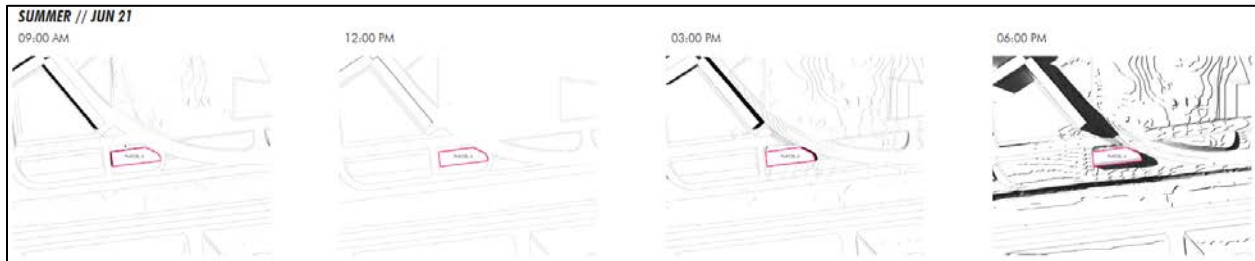
Area 6

As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 6 would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 30 feet (approximately three stories).
- Building setbacks would be:
 - 15-foot setback for potential buildings developed along McClean Parkway (eastern edge) and Georgetown Avenue (western edge);
 - 50-foot setback for potential buildings developed along Westchester Parkway (northern edge); and
 - Limited Development Area for the southern portion of Area 6 along Northside Parkway (southern edge).
- Buildings are prohibited within the Limited Development Area.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer would be negligible for all times of day. No shadow-sensitive uses would be shaded by the maximum proposed development of Area 6, as shown in **Figure 4.1-81**. Therefore, impacts related to shading for more than four hours between 9:00am and 5:00pm from early April to late October in Area 6 would not occur.

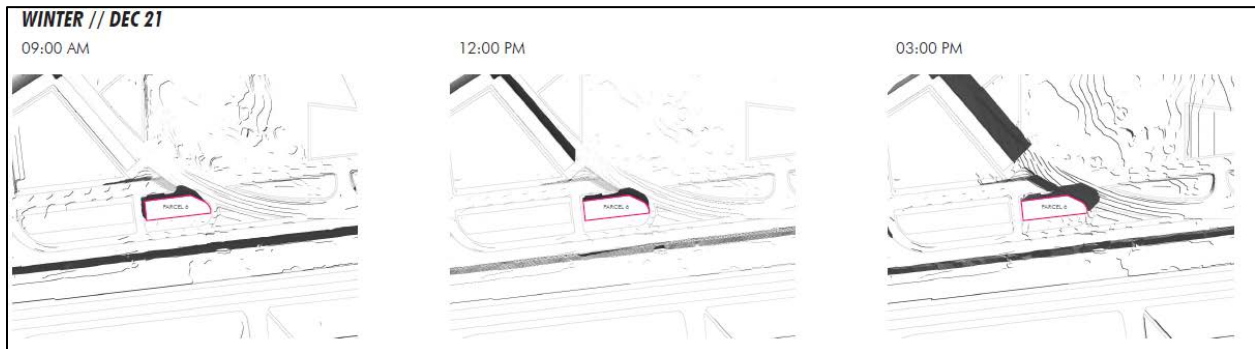
Figure 4.1-81 – Summer Shadows Cast by Proposed Maximum Development of Area 6



Source: Rios Clementi Hale Studios, 2014

During the winter, the proposed maximum development of Area 6 would cast shadows only on surrounding roadways: Georgetown Parkway, McClean Parkway, Westchester Parkway, and Northside Parkway. No shadow-sensitive uses would be shaded at any time, as shown in **Figure 4.1-82** below.

Figure 4.1-82 – Winter Shadows Cast by Proposed Maximum Development of Area 6



Source: Rios Clementi Hale Studios, 2014

Therefore, impacts related to shading for more than three hours between 9:00am and 3:00pm from late October to early April in Area 6 would not occur.

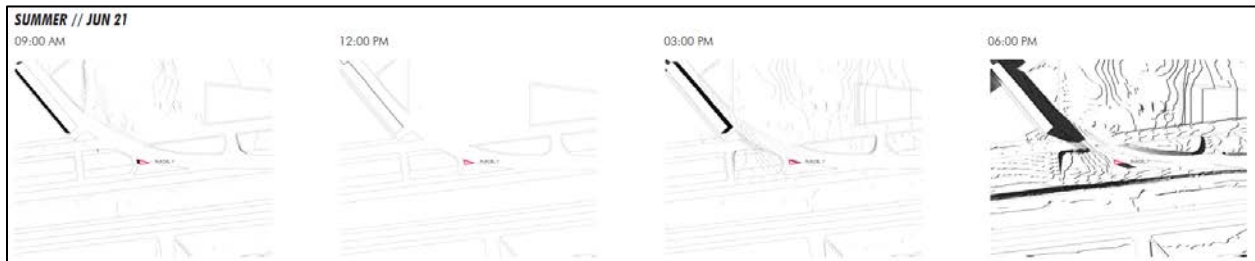
Area 7

As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 7 would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 30 feet (approximately three stories).
- Building setbacks would be:
 - 15-foot setback for potential buildings developed along the northern portions only of McClean Parkway (western edge) and Lincoln Boulevard (northern edge); and
 - Limited Development Area for the southern portion of Area 7 along Northside Parkway (majority of Area 7).
- Buildings are prohibited within the Limited Development Area.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer would be negligible for all times of day. No shadow-sensitive uses would be shaded by the maximum proposed development of Area 7, as shown in **Figure 4.1-83**. Therefore, impacts related to shading for more than four hours between 9:00am and 5:00pm from early April to late October in Area 7 would not occur.

Figure 4.1-83 – Summer Shadows Cast by Proposed Maximum Development of Area 7

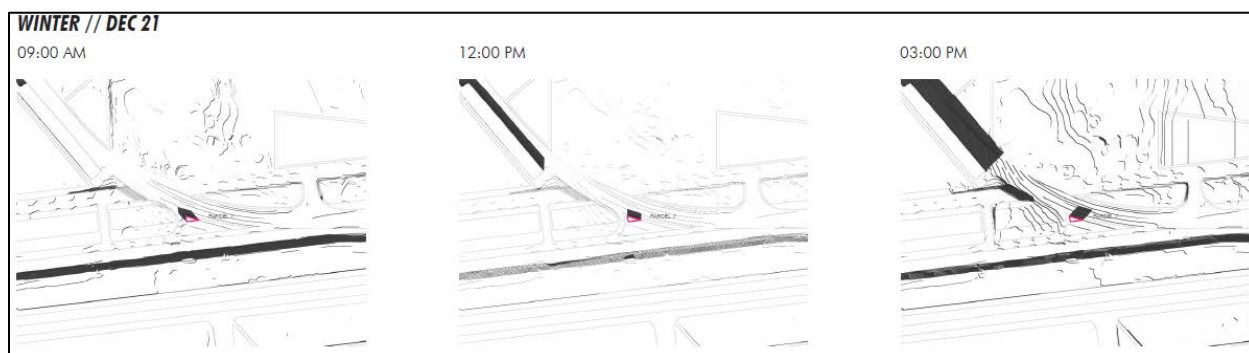


Source: Rios Clementi Hale Studios, 2014

During the winter, the proposed maximum development of Area 7 would cast shadows only on Lincoln Boulevard. No shadow-sensitive uses would be shaded at any time, as shown in **Figure 4.1-84** below.

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Figure 4.1-84 – Winter Shadows Cast by Proposed Maximum Development of Area 7



Source: Rios Clementi Hale Studios, 2014

Therefore, impacts related to shading for more than three hours between 9:00am and 3:00pm from late October to early April in Area 7 would not occur.

Area 8

As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51**, buildings in Area 8 would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 30 feet (approximately three stories).
- Building setbacks would be:
 - 15-foot setback for potential buildings developed along Lincoln Boulevard, Westchester Parkway, and McConnell Avenue/South McConnell Avenue (all edges of Area 8).

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer would be negligible at 9:00am, 12:00pm, and 3:00pm. At 6:00pm, after one hour after the close of the sensitive time frame (9:00am to 5:00pm), the proposed maximum development of Area 8 would cast a shadow on a portion of the Aircraft Surveillance Radar Facility in Area 9. Although this use is not shaded at 3:00pm, it may be shaded before or at 5:00pm. However, the Aircraft Surveillance Radar Facility is an airport support use and shading would have no impact on the function of the facility. It is not a shadow-sensitive use, and thus no shadow-sensitive uses would be shaded by the maximum proposed development of Area 8, as shown in **Figure 4.1-85**. Therefore, impacts related to shading for more than four hours between 9:00am and 5:00pm from early April to late October in Area 8 would not occur.

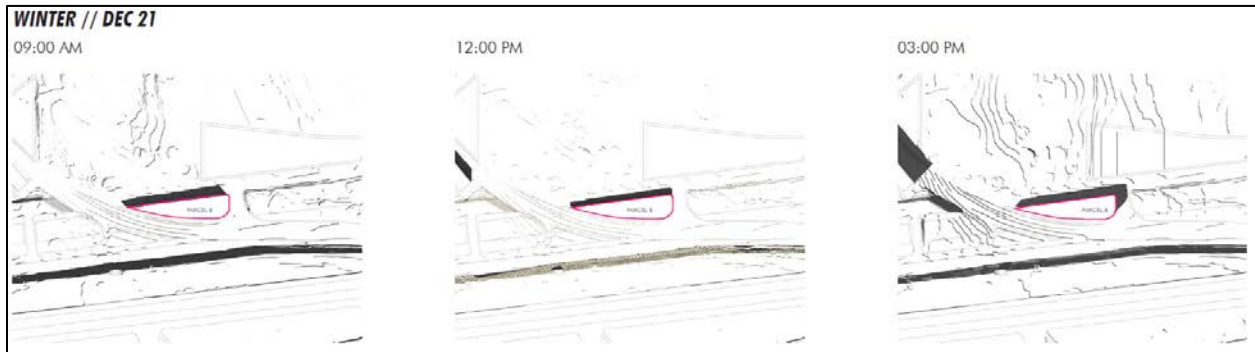
Figure 4.1-85 – Summer Shadows Cast by Proposed Maximum Development of Area 8



Source: Rios Clementi Hale Studios, 2014

During the winter, the proposed maximum development of Area 8 would cast shadows only on Westchester Parkway and McConnell Avenue. No shadow-sensitive uses would be shaded at any time, as shown in **Figure 4.1-86** below.

Figure 4.1-86 – Winter Shadows Cast by Proposed Maximum Development of Area 8



Source: Rios Clementi Hale Studios, 2014

Therefore, impacts related to shading for more than three hours between 9:00am and 3:00pm from late October to early April in Area 8 would not occur.

Area 9

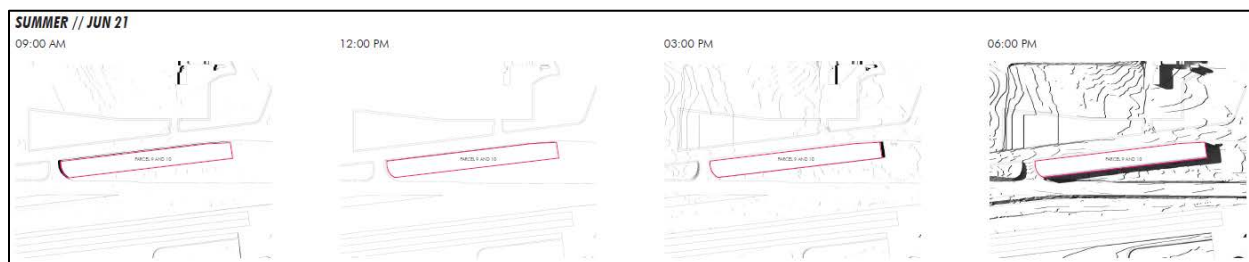
As described in the Project Design Features and shown in **Figure 4.1-50** and **Figure 4.1-51** above, buildings in Area 9 would have the following characteristics that would affect how much shading is generated by the proposed development:

- Maximum height for any development would be 30 feet (approximately three stories).
- Building setbacks would be:
 - 15-foot setback for potential buildings developed along McConnell Parkway (western edge) and the western portion of Westchester Parkway (western half of the northern edge); and
 - Limited Development Area for all of Area 9 except for the northwestern quadrant.
- Buildings are prohibited within the Limited Development Area.

Given the maximum building massing permitted by these physical restrictions, shadows cast by the proposed Project during the summer would be negligible for all times of day during the sensitive time frame. No shadow-sensitive uses would be shaded by the maximum proposed development of Area 9, as shown in **Figure 4.1-87**. Therefore, impacts related to shading for more than four hours between 9:00am and 5:00pm from early April to late October in Area 9 would not occur.

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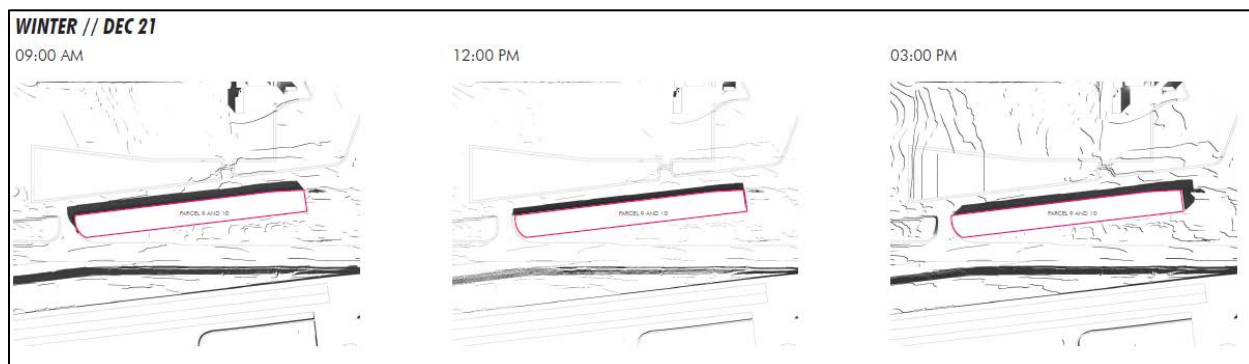
Figure 4.1-87 – Summer Shadows Cast by Proposed Maximum Development of Area 9



Source: Rios Clementi Hale Studios, 2014

During the winter, the proposed maximum development of Area 9 would cast shadows only on Westchester Parkway and McConnell Avenue. No shadow-sensitive uses would be shaded at any time, as shown in **Figure 4.1-88** below.

Figure 4.1-88 – Winter Shadows Cast by Proposed Maximum Development of Area 9



Source: Rios Clementi Hale Studios, 2014

Therefore, impacts related to shading for more than three hours between 9:00am and 3:00pm from late October to early April in Area 9 would not occur.

Area 10

The entirety of Area 10 would be classified as Limited Development Area, and therefore is prohibited from construction of new buildings. Therefore, impacts related to shading for four hours or more between the hours of 9:00am and 5:00pm between early April and late October or for three hours or more between the hours of 9:00am and 3:00 pm between late October and early April in Area 10 would not occur.

4.1.3.5.3 Transfer Program

The proposed Project would include flexibility to allow for transfers of floor area between uses within Districts on a per square foot basis. While transfers of floor area within Districts would be permitted, the maximum proposed Project total of 2,320,000 square feet may not be exceeded. Transfers of floor area would not result in new impacts with regard to aesthetics. All new proposed Project development (regardless of where land uses would specifically occur within the Project site) would incorporate the Project Design Features previously described, which include building heights and massing, building design, parking design, and proposed LAX Northside Design Guidelines and Standards. As such, transfers of floor area would not alter the

conclusions with regard to aesthetics impacts. Should uses be transferred within Districts, the resulting impacts would be similar to those evaluated herein.

4.1.4 Cumulative Impacts

The geographic context for the cumulative impact analysis of aesthetics, views, light/glare, and shading is the immediate Project vicinity, as such impacts are highly localized. Of future development through 2022 in the surrounding area, only those projects sufficiently close to influence the visual character of the immediate Project site area, fall within the same viewshed as the proposed Project, or affect the same off-site sensitive uses could pose cumulative effects in conjunction with the proposed Project.

As previously discussed, the Project site is located in a highly urbanized area that includes fully developed residential neighborhoods to the north, the LAX North Airfield to the south, commercial uses along Sepulveda Boulevard to the east, and LAX-owned open space to the west. The LAX-owned open space is part of the Los Angeles Airport/El Segundo Dunes located to the west of the Project site that will not include future development. As such, it will not contribute to cumulatively significant impacts to visual quality. Areas to the north, south, and east of the Project site are fully developed and urbanized. While redevelopment of these areas may occur, new uses would be of a similar visual character to existing residential, commercial, and airport uses. It is not anticipated that these related projects would introduce new aesthetic elements that would be out of scale or character with the existing visual environment. Therefore, cumulative impacts relative to visual quality would be less than significant.

Development of low-rise structures and lower intensity development anticipated in conjunction with ambient growth in the Project area would not be anticipated to have a substantial effect on views since the Project area is already highly urbanized. Given the limitations on such views under existing conditions, increased building heights and density associated with future growth would merely affect views from adjacent vantages and would have a negligible effect on longer-range views down roadways. Therefore, future development in the Project area would not be expected to cumulatively obstruct public views of valued visual resources.

Development of the proposed Project in combination with future developments in the proximity of the Project site through 2022 would introduce new or expanded sources of artificial light and thus could contribute to increased nighttime light levels as experienced by off-site sensitive uses. As previously described, the area around the Project site is a highly urbanized environment with urban lighting characteristics, exhibiting medium to high ambient nighttime light levels. As such, the additional artificial light sources introduced by the nearby related projects as well as other ambient growth would not significantly alter the existing lighting environment currently experienced in the area. Additionally, cumulative lighting would not be expected to interfere with the performance of off-site activities given the high ambient light levels already present. Further, proposed Project adherence to applicable guidelines regarding lighting, discussed above, would control the proposed Project's potential artificial light sources to a sufficient degree so as not to be considered cumulatively considerable. Therefore, development of the proposed Project would result in less than significant cumulative lighting impacts.

With regard to glare, only related development immediately adjacent to proposed Project structures would have the potential to create glare that could collectively pose impacts affecting a given off-site use, property, or activity. Development directly adjacent to the Project site is governed by FAA and Los Angeles County Airport Land Use commission guidelines that prohibit

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the use of highly reflective surfaces that could result in glare. As such, cumulative glare impacts would be less than significant.

Cumulative shading impacts can occur when related projects are located sufficiently close to a Project site so as to create shadows that overlap with those of the proposed Project and affect the same sensitive receptor(s). Sensitive receptors located adjacent to the Project site include residences directly to the north. There are no shade-sensitive receptors to the east, west, or south of the Project site. Given that there are no lands available to develop new projects between the Project site and adjacent sensitive residential receptors (i.e., residences are located directly adjacent to the Project site and are surrounded by other residences to the north, east, and west), impacts relative to shading would be less than significant.

4.1.5 Mitigation Measures

4.1.5.1 Visual Character – Aesthetics

As discussed above, implementation of the proposed Project Design Features and applicable LAX Master Plan EIS/EIR mitigation commitments would result in less than significant impacts to aesthetics. Therefore, no mitigation measures are required.

4.1.5.2 Visual Character – Views

As discussed above, implementation of the proposed Project Design Features and applicable LAX Master Plan EIS/EIR mitigation commitments would result in less than significant impacts to views. Therefore, no mitigation measures are required.

4.1.5.3 Light and Glare

As discussed above, implementation of the proposed Project Design Features and applicable LAX Master Plan EIS/EIR mitigation commitments would result in less than significant impacts related to light and glare. Therefore, no mitigation measures are required.

4.1.5.4 Shade and Shadows

As discussed above, implementation of the proposed Project Design Features and applicable LAX Master Plan EIS/EIR mitigation commitments would result in less than significant impacts related to shade and shadows. Therefore, no mitigation measures are required.

4.1.6 Level Of Significance after Mitigation

Impacts related to aesthetics would be less than significant without mitigation.